

EL MONTE

COMMUNITY DEVELOPMENT

DOWNTOWN

SPECIFIC PLAN



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TRANSIT-ORIENTED DISTRICT *Specific Plan & Master Plan*

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TRANSIT-ORIENTED DISTRICT
Specific Plan & Master Plan

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Chapter 1
**INTRODUCTION AND
BACKGROUND**



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1. INTRODUCTION AND BACKGROUND

1.1 OVERVIEW

The Downtown Main Street Transit-Oriented District Specific Plan and Master Plan (herein referred to as Specific Plan) was prepared in response to METRO grant funding allocated for future transit-oriented developments throughout the greater Los Angeles area. This Specific Plan has been prepared with an emphasis on providing standards and guidelines to revitalize the Downtown area of El Monte into a pedestrian-oriented environment with increased housing density, services, and other supporting land uses in close proximity to existing transit facilities.

The Specific Plan area encompasses approximately 115 acres within the City's Downtown area. This area has been a part of a multimodal transitioning process that has been ongoing for a number of years resulting in the creation of a regional transit hub. Located within or directly adjacent to the Specific Plan area, the El Monte Metrolink Station and El Monte Bus Station facilities have seen increased activity due to expanded services and a number of recently completed facility enhancements. Both of these transit facilities provide the Specific Plan area with a number of opportunities to diversify existing land uses within the Downtown including a broader range of housing, retail, entertainment, and outdoor dining establishments.

Of significant importance to the Specific Plan area is the roadway segment originally known as Main Street. In the 1930s, its name was changed to Valley Boulevard and in the mid-1960s its name was changed to Valley Mall (when Valley Boulevard was rerouted to the north to divert through traffic). This is El Monte's original central business district and its historical importance is embraced and celebrated within this Specific Plan through a number of proposed land use, mobility, and streetscape beautification enhancements. The first implementation measure of the Specific Plan called to rename Valley Mall to Main Street (Section 3.5). The renaming was accomplished in 2018 to much fanfare. This immediately conveyed to residents and visitors the historical importance of this roadway located in the heart of the City.

Overall, the primary purpose of this Specific Plan is to create a unique and identifiable Downtown core anchored by a Main Street that is an economically viable, vibrant, and pedestrian-friendly destination. This Specific Plan will act as the tool for implementing the overall vision of expanding El Monte's Downtown core, increasing land use intensities around existing transit facilities, and providing pedestrian-friendly mobility and streetscape improvements in support of transit-oriented developments throughout the Downtown area.

1.2 WHAT IS A SPECIFIC PLAN?

A specific plan is a tool provided by Section 65450 et seq. of the California Government Code for the systematic implementation of the City's General Plan. It establishes a link between implementing policies of the General Plan and individual development proposals within the defined boundary of the Specific Plan. While the General Plan is the primary guide for growth and development within El Monte, the Specific Plan focuses in more detail on the Downtown core area and surrounding neighborhoods. It tailors land uses and zoning regulations to accommodate a desired mix of uses with guidelines and standards thus creating a development form and composition that supports a vibrant, active Downtown shopping district and neighborhood.



Downtown El Monte looking east on Valley Mall (Main Street).

1 INTRODUCTION & BACKGROUND

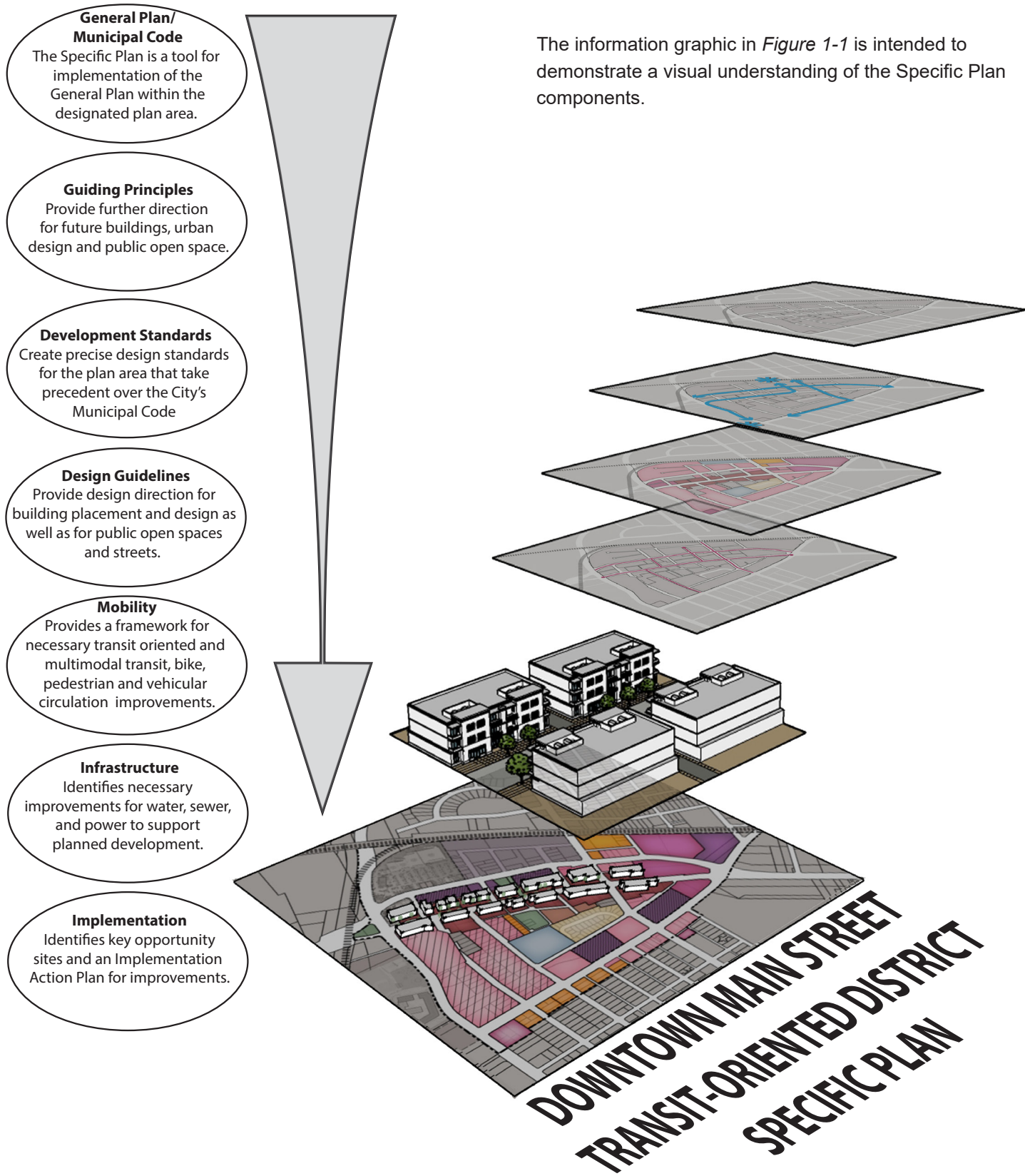


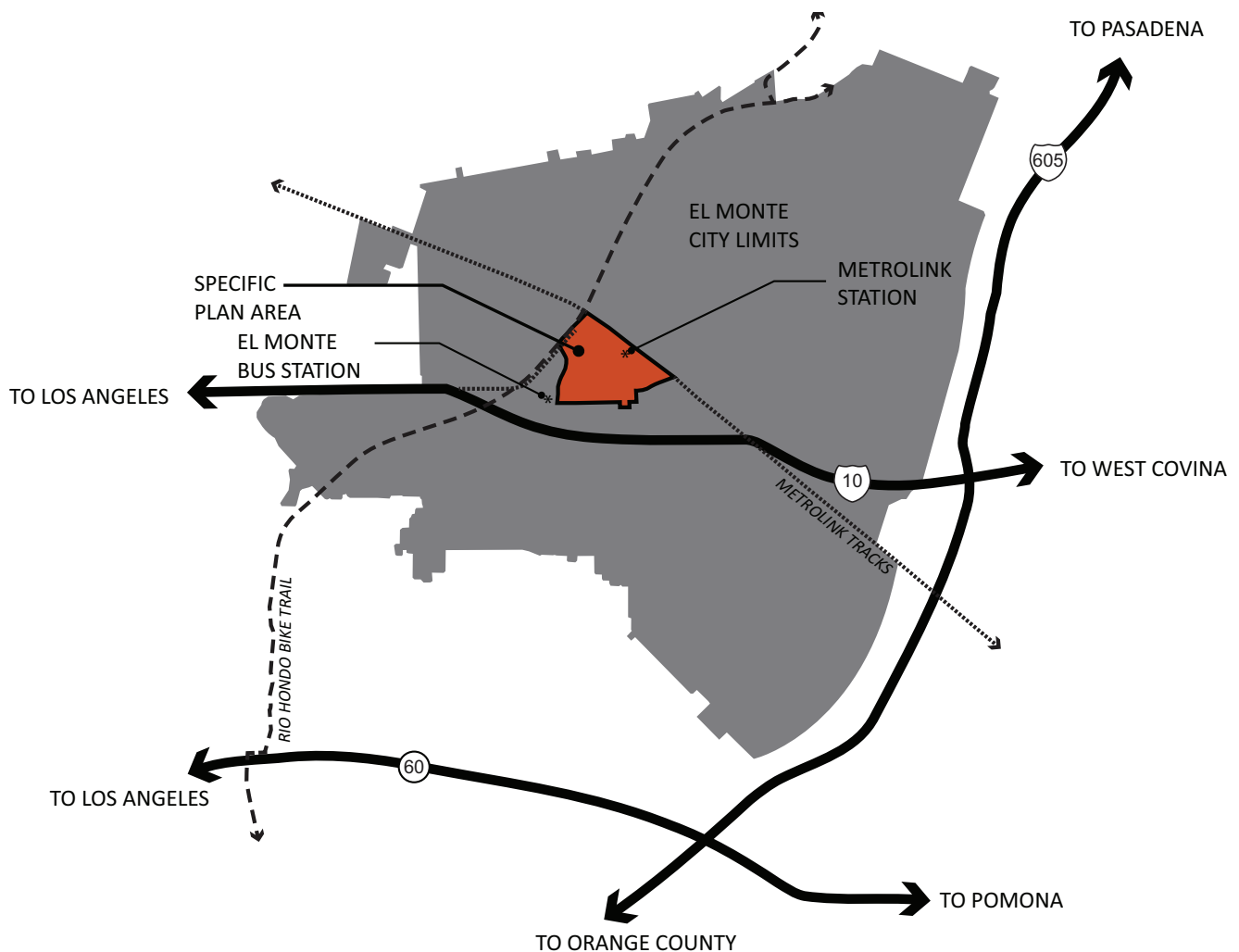
Figure 1-1 Framework of a Specific Plan

1.3 BOUNDARIES AND SETTING

1.3.1 REGIONAL CONTEXT

The City of El Monte is nestled in the San Gabriel Valley, about 14 miles east of Downtown Los Angeles. El Monte is located in close proximity to other Los Angeles communities, such as Rosemead and San Gabriel to the West, South El Monte to the South, West Covina and Baldwin Park to the East and Arcadia and Temple City to the North. Downtown El Monte also takes advantage of access to the Metrolink Train Station, Metro Bus Station and Rio Hondo River Bike Trail. Although El Monte is located in one of Southern California’s largest metropolitan areas, the Downtown core has a distinct

character making it unique to the Los Angeles basin. El Monte’s Downtown, as defined by the General Plan⁽¹⁾, encompasses a gross area of 200 acres, generally bordered by the Rio Hondo River, Interstate 10 and the Metrolink and Union Pacific train tracks. The Specific Plan boundary is in close proximity to Metrolink Transit Routes and Interstate 10, which provides residents easy access to Downtown Los Angeles and surrounding communities, see *Figure 1-2*.



(1) "Downtown Core" land use designation

Figure 1-2 Project Context Map

1.3.2 DOWNTOWN CONTEXT

As described in El Monte’s General Plan, the Downtown is currently comprised of five (5) sub-districts. These sub-district are named the El Monte Gateway, Main Street, Government Center, Downtown Residential, and Cultural Center (Figure 1-3). This Specific Plan is comprised mainly of the Main Street Sub-District and includes the northern most part of the Downtown Residential sub-district along Ramona Boulevard. The following is a summary of the General Plan Sub-Districts:

Main Street: The Main Street Sub-District is included within the Specific Plan area and includes Main Street (Valley Mall) with several blocks of street-facing retail and tree lined sidewalks.

Government Center: Consists of City Hall, County Superior Court, educational uses and the El Monte Police Station.

Cultural Center: The Cultural Center is located south of the Specific Plan area and is home to the El Monte Community Center, Historical Society Museum, Jack

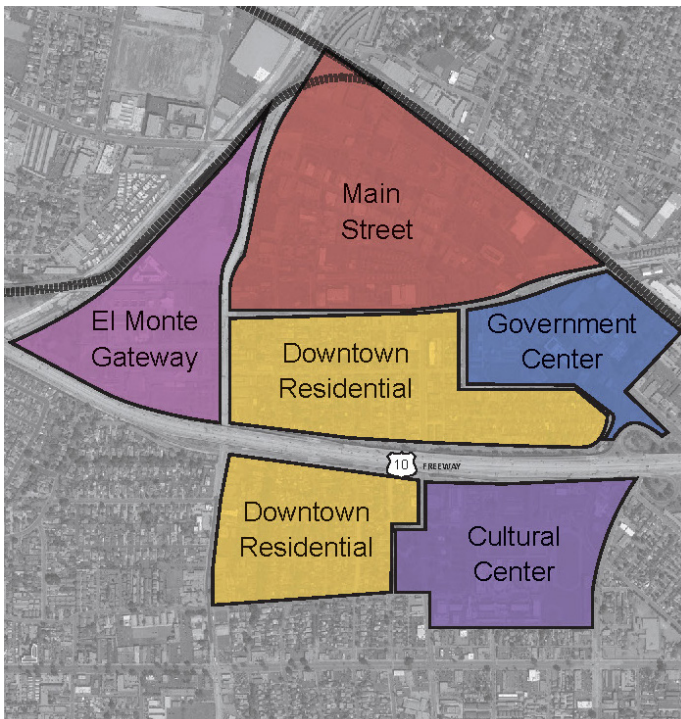


Figure 1-3 Downtown Area Sub-District Context Map

Crippen Senior Center, Aquatic Center, El Monte High School, and Tony Arceo Memorial Park. Many street fairs, concerts and community gatherings occur within the Cultural Center.

Downtown Residential: Various smaller well established neighborhoods make up the southern portion of the Downtown creating unique identities in each residential area.

El Monte Gateway: The Gateway Specific Plan was adopted in 2007 and updated in 2022 for a 60-acre mixed-use community project directly west of Downtown El Monte. It will integrate a mix of transit, housing, retail, business, entertainment, and public parks centered around the El Monte Bus Station. A 133-unit affordable housing project (The Exchange at Gateway) was completed in 2015 and a mixed-use project (the Aston at Gateway) with 25,000 square feet of ground floor retail and 208 upper floor apartment units is anticipated to be completed in 2023. The Specific Plan also calls for improved access to the El Monte Bus Station. The streetscape improvements proposed along the west side of Santa Anita Avenue are in alignment with the public-right-of-way improvements envisioned along the east side of Santa Anita Avenue within Downtown El Monte.

Source: El Monte Gateway Specific Plan



Figure 1-4 El Monte Gateway Specific Plan

1.3.3 SPECIFIC PLAN AREA SETTING AND BOUNDARY

For decades, Main Street (later changed to Valley Boulevard in the 1930s) was considered one of the main shopping hubs between Pasadena and Pomona. In the 1950s, a parking district was formed and public parking lots were constructed to support Downtown businesses. In the mid-1960s, Valley Boulevard was rerouted to the north to divert through traffic, and the Downtown segment of Valley Boulevard was renamed Valley Mall. In order to improve the pedestrian experience, Valley Mall’s roadway was narrowed, sidewalks were widened and landscaping was added. However, by the early 1980s, new enclosed shopping centers in Arcadia, Montebello and West Covina began to attract El Monte customers and Valley Mall began to decline. Nonetheless, Valley Mall continued to hold an important place in the community through the 2000s.

For the past decade, Downtown El Monte has seen several new investments and improvements. Examples include the new 100,000 square foot Santa Fe Trail Plaza (completed in 2015), the refurbished Tiny Tot Lot playground (2016), the Norms Restaurant (2018) and the weekly Main Street Farmers Market (beginning in 2020). Housing projects include the 62-unit Union Walk project (completed in 2016), the 106-unit Cesar Chavez projects (to be completed in 2023), the 50-unit Domus project (to begin construction in 2022) and the 85-unit Urban Pacific project (under review in 2022). The Downtown Main Street Transit-Oriented District Specific Plan and Master Plan’s vision seeks to continue reinvigorating the Downtown area by supporting existing businesses, attracting new retailers and eateries and improving mobility connections.

The Specific Plan covers an area of 115 acres (*Figure 1-5*) and is bounded by railroad tracks to the North, Santa Anita Avenue to the west, and Ramona Boulevard to the south.

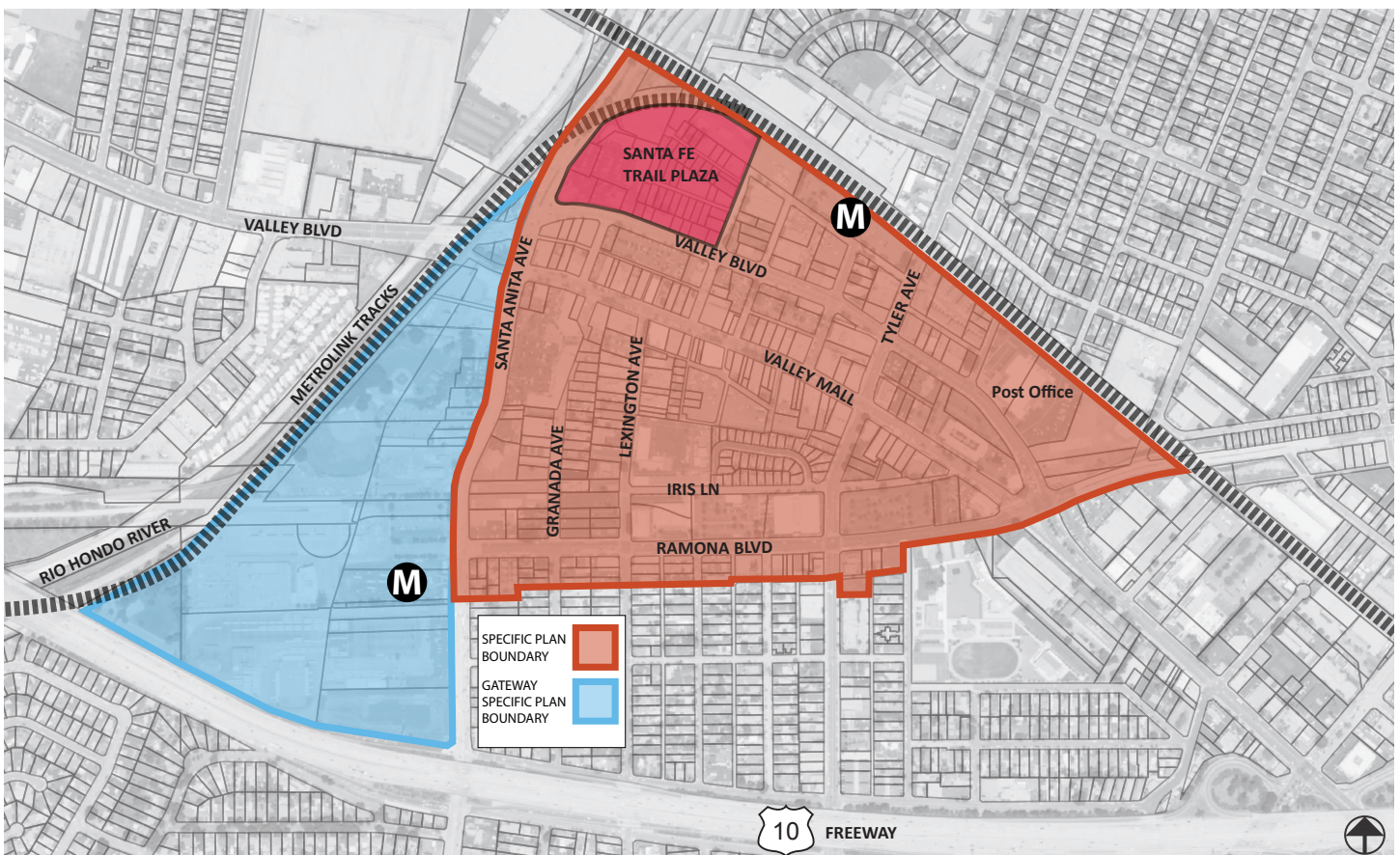


Figure 1-5 Specific Plan Area

1.4 RELATIONSHIP TO OTHER PLANNING DOCUMENTS

This Section examines several existing documents, policies, and programs that provide direction for the future of the Specific Plan area. Relevant documents, policies, and studies are summarized below.

1.4.1 “VISION EL MONTE” 2011 GENERAL PLAN

The Specific Plan is consistent with and furthers the objectives and policies of the “Vision El Monte” 2011 General Plan (June 2011) by providing detailed criteria for the development of specific sites and public streetscape improvements. The Specific Plan’s vision, goals, and implementation measures are based on direction given in the City’s General Plan. The Specific Plan provides for a more precise implementation of the General Plan’s goals, objectives, and policies. The following is a summary of the primary goals that capture the vision of the Specific Plan.

GOAL CD-2: Attractive commercial corridors exemplified by consistency of hardscape, landscaping, signage, sidewalks, and other treatments appropriate to their context to foster a pleasant driving and pedestrian experience.

GOAL CD-5: A vibrant Downtown that is an attractive, accessible and pedestrian-friendly center noted for its wide range of quality shopping, entertainment, and, cultural and recreational amenities.

GOAL LU-9: Recreate vibrant commercial corridors with the introduction of a mix of higher density residential uses, sensitively integrated commercial uses concentrated at critical nodes, and tree-lined streetscapes that are aesthetically pleasing, encourage walking, and inspire community pride.

GOAL C-4: A local and regional transit service that is accessible and safe; connects to homes, residences, parks, and other community destinations; and provides a viable alternative to the use of autos.

GOAL C-4: Integration of circulation and land use development policies and practices that support walking, bicycling, and use of transit through a variety of supportive land use development and urban design measures.

1.4.2 EL MONTE ZONING ORDINANCE

The City of El Monte Zoning Ordinance provides standards for site-specific development and land use regulations that govern the size, shape and type of use for development in the City of El Monte. This Specific Plan document customizes the standards and regulations found in the City Zoning Ordinance to help implement the Specific Plan. In any instance where the Specific Plan conflicts with the requirements of the Zoning Ordinance, the Specific Plan provisions will take precedence. **Where the Specific Plan is silent on a topic, the City of El Monte Zoning Ordinance requirements remain in force.**

1.4.3 EL MONTE GATEWAY SPECIFIC PLAN

The El Monte Gateway Specific Plan is located directly west of the Specific Plan area, across Santa Anita Avenue, and establishes policy and regulatory guidance for all properties with the intention of creating a vibrant, mixed-use urban activity center. The Gateway Specific Plan’s recommendations have been referenced and considered as part of the recommendations found within this document.

1.4.4 LOS ANGELES COUNTY AIRPORT LAND USE PLAN

The San Gabriel Valley Airport located in the City of El Monte is a public airport owned and operated by the County of Los Angeles. The County of Los Angeles’s Regional Planning Commission acts as the Airport Land Use Commission (ALUC) for coordinating the airport planning of public agencies within the county. The Specific Plan area is directly adjacent to the ALUC planning boundaries and does not require any oversight on new development from the ALUC.

1.4.5 EL MONTE AIRPORT MASTER PLAN

The Specific Plan is consistent with the El Monte Airport Master Plan which provides noise and safety compatibility requirements as well as land use plans and policies for the El Monte Airport (now referred to as the San Gabriel Valley Airport), located northwest of the Specific Plan area (Figure 1-6). Most of the Specific Plan area remains

outside of the 65-CNEL contour, which is the maximum acceptable noise exposure for residential land uses. The Specific Plan is consistent with the goals of avoiding residential and other noise-sensitive development in this zone specified in the El Monte Airport Master Plan.



Figure 1-6 El Monte Airport Noise Zones

1.4.6 EMERALD NECKLACE EXPANDED VISION PLAN

The Emerald Necklace Expanded Vision Plan envisions connecting the Los Angeles region through an integrated network of bicycle and pedestrian pathways, spanning from the Angeles National Forest to the north with the Pacific Ocean and Catalina Island to the west. A portion of the Emerald Necklace runs directly adjacent to the Specific Plan area along the Rio Hondo River. This Specific Plan has referenced and integrated policies and guidelines consistent with the Emerald Necklace Expanded Vision Plan and is discussed in more detail in **Section 3.4** of this document.

1.5 COMMUNITY OUTREACH PROCESS

Through the Specific Plan process, a series of meetings and workshops were conducted to receive community input and identify and prioritize goals and desires within the community. Input received throughout the outreach process was used to inform the vision, policies, standards, and guidelines of the overall Specific Plan area, such

as outdoor dining and public amenities. Members of the Specific Plan team conducted extensive outreach efforts that included Ad-Hoc Committee meetings, public workshops, and stakeholder interviews to identify goals and objectives for Downtown El Monte. The following is a summary of a number of outreach events and information received.

1.5.1 KEY STAKEHOLDER MEETINGS

The Specific Plan team conducted interviews with agencies, organizations, and individuals from the community. These included Day One, Amigos de los Rios, Bike SGV, the El Monte School District, as well as business owners, developers, and merchant groups such as the Chamber of Commerce and the Downtown El Monte Business Association (DEMBA). The interviews proved to be helpful in gaining input and insight from community members' issues and desires. Some of the common themes included the concern with safety at nighttime, an interest in providing opportunities for outdoor dining and limited sidewalk commercial, the provision of signage opportunities, an interest in additional retail and a variety of restaurants, and a desire to improve connectivity between the El Monte Metrolink Station and El Monte Bus Station.

1.5.2 COMMUNITY WORKSHOP #1

On March 3, 2014 the first community workshop was held and an overview of the Specific Plan process, including public outreach, plan preparation, and public hearings, was presented. Community members in attendance were provided with an overview of Transit-Oriented Development and the associated positive impacts that higher density developments have had on other communities when placed in close proximity to transit hubs. The public was then given a clear description of the Specific Plan area boundary along with current projects already ongoing in and around the Downtown area. At the end of the workshop, community members in attendance were placed in groups of five to ten people and asked to participate in a design exercise intended to gather comments and ideas of what they would like to see incorporated into the Specific Plan area. Participants then

summarized their group comments and presented them to the entire audience. Common themes that emerged from this design exercise included:

- *Adding a central park/plaza within Downtown;*
- *Providing new retail spaces;*
- *Encouraging more outdoor dining; and*
- *Integrating additional streetscape beautification improvements.*

1.5.3 COMMUNITY WORKSHOP #2

On June 12, 2014, members of the Specific Plan team and City staff facilitated the second community workshop for the project. Community members in attendance were provided with an update on meetings and activities that had taken place since the first workshop was held and a summary of the work completed on the project to date. The planning team also presented the results from the existing baseline studies conducted on the Downtown area and provided suggestions for land use changes, opportunity areas, and an increase in density around the existing transit stations. A number of graphics representing potential pedestrian, circulation, and streetscape improvements within the Specific Plan area were displayed and comments provided by the audience were discussed. Common themes brought up by community members in attendance included:

- *Enhancing Downtown connectivity to surrounding neighborhoods;*
- *Providing additional improvements along Main Street;*
- *Integrating better signage and parking solutions; and*
- *Increasing the amount of affordable housing.*

1.5.4 AD-HOC COMMITTEE MEETINGS

In addition to the community workshop meetings, a series of presentations were made to the City's Ad Hoc Committee. The Committee consisted of two City Council members, the City Manager, the Community and Economic Development Director, and the Community and Economic Development Deputy Director, who meet as needed to review and discuss proposed development projects in the City. The first meeting was held prior to the

commencement of community outreach meetings, with the other meetings held after the community workshops in order to provide an update on the findings and proposals for the Specific Plan.

Ad-Hoc Meeting #1: February 2014

This was the first of four planned Ad-Hoc Committee meetings. Topics covered included an overview of local points of interest such as the El Monte Bus Station, the pending Gateway Specific Plan area development with several hundred residential units, the parking surplus in Downtown, and the need for higher density housing, entertainment uses, and restaurants within Downtown. Staff and City Council members expressed expectations that the Specific Plan address:

- *Incorporating community & stakeholder input;*
- *Enhancing the programming of public spaces such as the "Old-Town Main Street that builds on the history of El Monte", through the definition of architectural character & style;*
- *Increasing density within Downtown;*
- *Capacity enhancement;*
- *Management strategies;*
- *Integrating context appropriate parking strategies; and*
- *Addressing the challenges and opportunities for Downtown through realistic strategies.*

Ad-Hoc Meeting #2: March 2014

The planning team presented an overview of the project scope to the ad-hoc committee and explained the benefits of using a Specific Plan as a vision for the community as well as an implementation tool for the creation of that vision. The presentation concluded with a request for guidance and input from the Ad-Hoc Committee on the Specific Plan progress. Both staff and City Council members expressed the following:

- *Enhancing connections to the Gateway Specific Plan project;*
- *Researching closing off Main Street to vehicular traffic entirely;*
- *Ensuring accommodation of outdoor dining and movie theatre uses within Downtown;*
- *Integrating design standards and guidelines that allow for national retailers;*

- *Creating an inviting environment that attracts both residents and visitors;*
- *Increasing the number of pedestrian paseos;*
- *Integrating urban parks;*
- *Restricting auto uses;*
- *Pedestrian signage;*
- *Allowing for sidewalk sales;*
- *Providing wayfinding signage;*
- *Parking capacity and management;*
- *Providing congregation areas for community events; and*
- *Enhancing existing Downtown buildings through façade improvements.*

Ad-Hoc Meeting #3: March 2015

At the third ad-hoc meeting, the planning team summarized the results of Workshops #1 and #2 and outlined work completed-to-date. A presentation of an abbreviated version of the Specific Plan Vision and Design Principles was provided and comments from the committee were discussed.

Ad-Hoc Meeting #4: April 2015

The purpose of this meeting was to provide an overview of the project scope and more specific details about components of the Specific Plan. Topics presented included:

- *Opportunities & constraints for the project;*
- *Proposed sub-areas within the Downtown area;*
- *Refined guiding principles for the Specific Plan;*
- *Overall vision for Main Street;*
- *Public plazas and green space network;*
- *Concepts for paseo and street network; and*
- *Existing parking conditions.*

The planning team ended the meeting by presenting the Ad-Hoc Committee with an outline of the next steps for the project, including the Environmental Impact Report (EIR).

1.5.5 ECONOMIC FORUM

The consultant Kosmont completed an economic analysis of the draft Specific Plan and drafted the Development

Opportunity Reserve (DOR) incentive methodology for the Specific Plan. In April 2016, staff hosted two economic forums with developers, architects, builders, real estate professionals, and entitlement managers. This was to solicit feedback and ensure the draft document was economically feasible and would encourage future development. Their comments were factored in the final recommendations.

1.5.6 CEQA RELATED MEETINGS

An initial environmental study was prepared in accordance with CEQA and made available to the public on July 31, 2015. A public scoping meeting was conducted on August 13, 2015 at City Hall. Over 25 people attended, most of whom were residents and business owners within the Specific Plan area. Those in attendance raised issues regarding recreational opportunities, traffic, eminent domain, property values, and the adoption process. The Draft EIR was circulated for public review from December 22, 2016 to February 6, 2017.

1.5.7 PLANNING COMMISSION AND CITY COUNCIL MEETINGS

A study session with the City Council was conducted in March 2015 to give an overview of the Specific Plan and the process. The City Council directed staff to proceed with the Specific Plan preparation and associated environmental review. A study session with the Planning Commission was conducted on August 25, 2015 to give an update on the Specific Plan and the process. The Planning Commission held public hearings on January 17, 2017 and February 28, 2017 and recommended the City Council certify the Final EIR and adopt the Specific Plan. The City Council held a public hearing on April 4, 2017 and certified the Final EIR and adopted the Specific Plan. As part of the City's Comprehensive Zoning Code Updates, the City Council held a public hearing on June 7, 2022 and approved an update to the Specific Plan.

1.6 VISION AND GUIDING PRINCIPLES

The vision for Downtown El Monte has been one that has been continually evolving over the past 100 years. This Specific Plan builds upon the existing historical context of the City while at the same time incorporating ongoing planning efforts to create a Downtown vision that emphasizes transit-oriented strategies, multi-modal options, and public realm enhancements. Through the interactive design exercises, stakeholder interviews, and community feedback, the importance of urban design, economics, and social capital were identified by the community as crucial elements necessary to make the Downtown a beautiful, safe, historic, culturally-rich, and lively regional destination.

“Vision El Monte...a community of beauty, with greenways, stands of trees, and flowers; a sense of place, with distinct and well-designed neighborhoods and districts; a community defined by and linked to its rivers, schools, parks and the downtown through attractive paths and streets-a destination.” - City of El Monte General Plan

1.6.1 GUIDING PRINCIPLES

The guiding principles outlined below were developed in response to feedback received during the community outreach process and are intended to inform the ultimate vision and subsequent development that will occur within the Specific Plan area.

1. Mixed-Use, Pedestrian, and Transit-Oriented Urban Village

Create a mixed-use, pedestrian and transit-oriented urban village focused around a pedestrian-friendly Main Street containing a mix of shops and supportive uses woven together by attractive streetscape improvements and paseos connecting to adjacent neighborhoods and transit centers.

2. Central Shopping and Entertainment District

Transform the Downtown into a central shopping and entertainment district that contains retail stores, neighborhood services, office spaces, and restaurants with outdoor dining; complemented by



Pedestrian-Friendly Main Street.



Downtown Paseos.



Outdoor Dining.



Variety of Housing Opportunities.



Downtown Plazas.



Transit-Oriented Developments.

broad sidewalks, plazas and parks designed with music and performance areas for resident and visitor enjoyment.

3. Enticing Place for Investment

Make Downtown an enticing place for investment by designing an active and vibrant street environment that attracts new businesses Downtown and by providing incentives to developers, downtown merchants, and property owners to locate in this area

4. Variety of Housing Opportunities

Incorporate a variety of housing opportunities throughout the Downtown that include a mix of densities and product types that use engaging architecture to further define El Monte’s historic Downtown.

5. Expanded and Improved Public Transit System

Create a Downtown supported by an expanded and improved public transit system, including a new trolley route throughout the Downtown core with connections to El Monte Bus Station and Metrolink Station.

6. Blend of Old and New

Provide a blend of old and new, where new development takes its design cues from the existing El Monte culture, character, and history. Define unique public plazas and green-space improvements connected by distinctive streetscapes interconnected with pedestrian paseos.

7. Balanced System of Multimodal Streets

Provide a well-connected and balanced system of multimodal streets (bus, auto, bike) connected to pedestrian linkages designed to accommodate all users regardless of age or ability.

8. Entryways at Key Intersections

Create entryways at key intersections to denote entries into the Specific Plan area through improved streetscapes and signage.

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DOWNTOWN
MAIN STREET

Chapter 2
LAND USE



Chapter 2 - Land Use

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2. LAND USE

2.1 INTRODUCTION

The future urban form of Downtown El Monte will be established by providing opportunities for development that adheres to the following customized mix of land uses, context-sensitive development standards, and placemaking design guidelines. The combination of these elements works to implement the vision for Downtown El Monte as described in Chapter 1.

The provisions of this Chapter apply to building additions, exterior remodels, relocations, or new construction requiring a building permit within the Specific Plan area.

2.1.1 HOW TO USE THIS CHAPTER

This Chapter sets the regulatory and design framework for developers, designers, city staff, and review bodies to develop, critique, and implement projects as they proceed through the entitlement process. The following steps are recommended to ensure project consistency with the contents provided herein:

1. *Principles* - Applicants should review and apply the Guiding Principles provided in **Section 1.6.1** as applicable. Reviewers should ensure the project is consistent with the intent of the Guiding Principles.
2. *Uses* - Applicants should review the list of permitted uses in Table 2-3 to ensure the proposed use is allowed.
3. *Area Wide Development Standards - Section 2.3* includes standards/regulations that apply to all sub-areas. Applicants must review and incorporate all applicable requirements. Reviewers shall ensure consistency between the project and the regulations/standards as applicable. In addition, all applications shall comply with the El Monte Zoning Ordinance as well as those documents outlined in **Section 1.4**.

4. *Sub-Area Specific - Sections 2.4 - 2.7* include tailored standards/regulations that serve as the zoning regulations for that sub-area. Applicants must comply with all applicable regulations. Reviewers shall ensure consistency between the project and the regulations/standards as applicable.
5. *Design Guidelines* - Designers should comply with the intent of the El Monte Comprehensive Design Guidelines and the guidelines that have been provided in **Section 2.8** of this document. Reviewers should ensure compliance with the design guidelines.



Existing urban form along Main Street.



Corner retail opportunity along Main Street.

2.2 SUB-AREAS

The Specific Plan area is divided into four (4) sub-areas. Each unique sub-area includes a customized set of permitted land uses and development standards. Sub-area boundaries were developed by considering existing land uses, streets, parcel size, proximity to various community-wide destinations, and future development potential. Applicants should identify which sub-area their project is located within and refer to the development standards that apply to their project. The sub-area boundaries are shown in *Figure 2-1* and are described as follows:

Main Street Sub-Area

The Main Street Sub-Area is the economic, cultural, and historical “heart” of El Monte. It includes many existing mom-and-pop retail and dining establishments. Much of Main Street has been improved with wide sidewalks, street furnishings, and pockets of enhanced landscaping. A network of alleys and paseos allow for access to/from parking areas and adjacent sub-areas.

The Main Street Sub-Area is envisioned to be revitalized with façade improvements and vertical mixed-use development with residential or office above retail and restaurants. Buildings up to three stories in height will line Main Street and buildings up to four stories in height will front along Valley Boulevard.

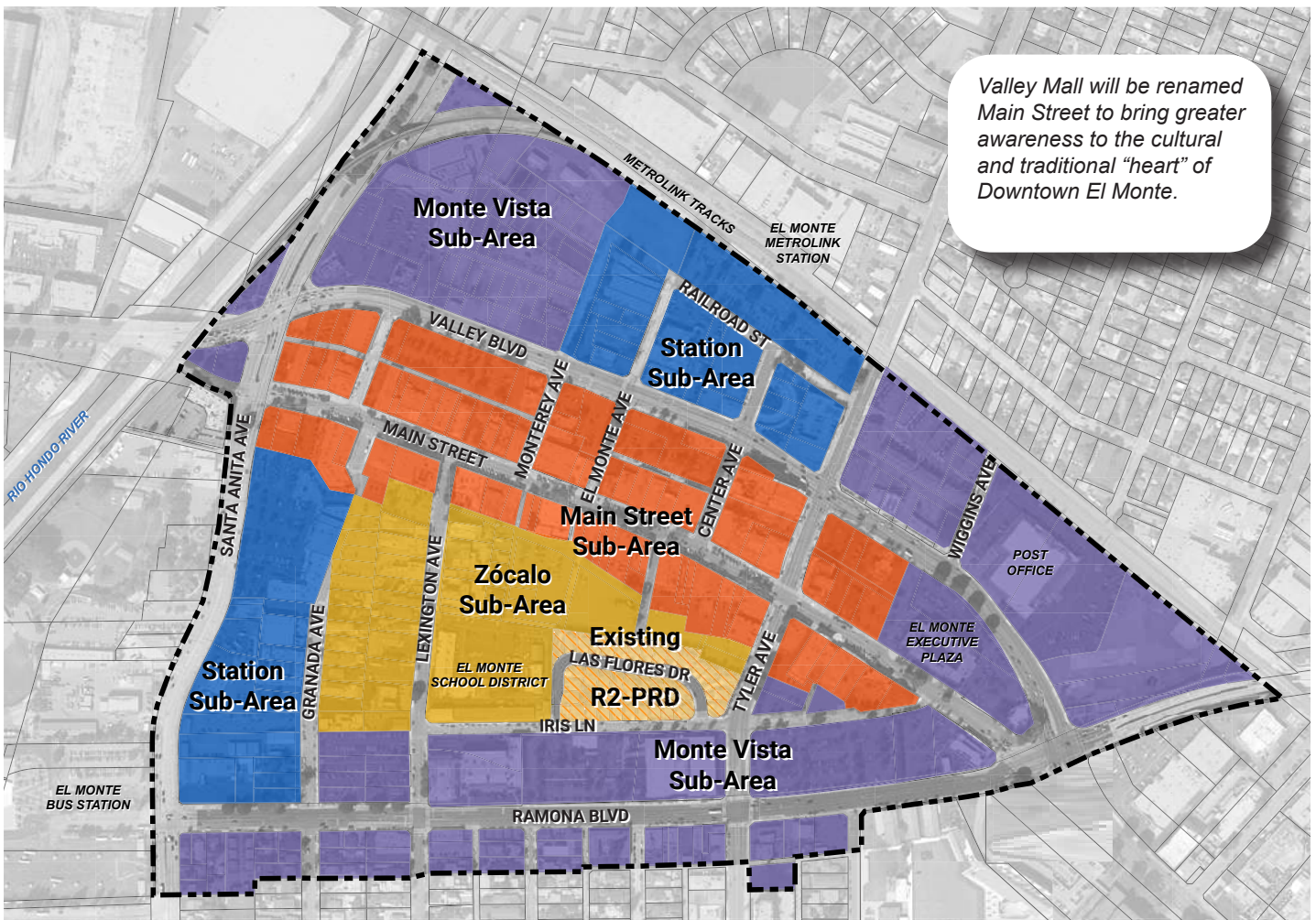


Figure 2-1 Specific Plan Sub-Area Map

Zócalo Sub-Area

Zócalo means “public square” which is a term that characterizes the physical location of this sub-area within Downtown El Monte. Today this Sub-Area includes mostly residential, retail, and office uses that are linked to the Main Street Sub-Area via paseos, alleys, and Lexington Avenue. One large centrally located parking area is utilized for Downtown events and provides parking for the businesses along Main Street.

The Zócalo Sub-Area is envisioned to transform underutilized parking areas into retail and housing opportunities centered around outdoor open space. This plaza will provide space for community arts and cultural events to occur. Artist live/work units in addition to multi-family residential developments up to five stories in height will support Main Street retail uses a short walk away. Along Lexington Avenue, retail and professional offices will provide an extension of Main Street uses and support the surrounding residences with various job opportunities.

Station Sub-Area

The Station Sub-Area embodies the transit-oriented elements of the Specific Plan area. Broken into two areas, the Station Sub-Area includes the El Monte Metrolink Station and an adjacency to the El Monte Bus Station which serve as regional transit hubs for the City. Both transit stops are within a comfortable 5-10 minute walking distance to Downtown restaurants, shops, and services.

The Station Sub-Area is envisioned to include a complementary mix of retail, urban housing, and transit uses. Stand-alone multi-family residential uses such as townhomes, studio flats, and apartments up to six stories will provide housing opportunities for transit users in close proximity to Downtown. To complement residential uses, small retail and visitor serving shops will provide an appropriate transition to the retail establishments of Main Street.

Monte Vista Sub-Area

The Monte Vista Sub-Area is located along the southern and northern edges of the Specific Plan area and is comprised of office, residential and public uses, including the El Monte United States Post Office. The Sub-Area



Stores with engaging displays, bike parking, and outdoor seating are encouraged in the Main Street Sub-Area.



Residential units above retail are encouraged in the Main Street and Zócalo Sub-Areas.



Access to transit and close proximity to shops are encouraged in the Station Sub-Area.

contains Downtown supporting uses and creates a buffer to surrounding residential uses.

The Monte Vista Sub-Area will be enhanced with street corridor beautification, mixed-use development, and Downtown entry treatments in key locations. New development is limited to a maximum of four stories to ensure an appropriate transition from the Downtown core to areas outside the Specific Plan boundary.

R2-PRD

The area identified on *Figure 2-1* as “Existing R2-PRD” is a well established residential neighborhood that is unlikely to change during the life of this Specific Plan. This area is exempt from the regulations and guidelines found herein and would follow the R-2 standards in Title 17 of El Monte’s Zoning Ordinance. However, right-of-way beautification improvements along Iris Lane shall comply with the provisions found in Chapter 3, Mobility and Beautification, of this Specific Plan.

OS

The area identified on *Figure 2-1* as “Existing OS” is land developed as Veterans Memorial Park. The area is improved with a statue recently dedicated to military veterans and the flags of the five major military branches. This area is exempt from the regulations and guidelines found herein and would follow the OS standards in Title 17 of El Monte’s Zoning Ordinance. However, right-of-way beautification improvements along both Valley Boulevard and Santa Anita Avenue shall comply with the provisions found in Chapter 3, Mobility and Beautification of this Specific Plan.

2.2.1 URBAN FABRIC

Figures 2-2 and 2-3 include a summary of key development standards that illustrate the desired urban fabric of Downtown El Monte. These elements are represented through building height, setbacks, relationship to the street, walkability, and land uses.

Refer to Sections 2.3 - 2.7 for a comprehensive overview of applicable development standards.

- *Figure 2-2* and Table 2-1 provide building density, height, and number of permitted stories within each sub-area. A range is provided via a Development Opportunity Reserve (DOR) that encourages developers to provide amenities and/or infrastructure improvements in exchange for increased building height, density, and stories (see **Section 2.2.2**).
- A building’s relationship to the street can be defined through setbacks and front entrance orientation. *Figure 2-3* includes street setback regulations for each sub-area. In some cases, a range is provided to require additional setbacks for on-site parking adjacent to a public street or for the provision of plazas, outdoor dining, and/or expanded sidewalks. The Main Street Sub-Area and portions of Lexington Avenue include 0’ minimum street setbacks to encourage building frontages adjacent to the sidewalk.
- Walkability is defined as the distance a person is willing to walk to access transit; typically a 5 to 10 minute distance. *Figure 2-2* illustrates a typical 5 minute walking radius from both the El Monte Metrolink and El Monte Stations. It should be noted that the majority of the Specific Plan area is within a comfortable 5-10 minute walking distance.
- A variety of land uses will add to the vibrancy and economic success of Downtown El Monte. In some instances, the location of the use is regulated to ensure the proper mix of uses are provided at the street level. *Figure 2-2* identifies locations where residential development is prohibited from fronting the street. In these areas, residential uses may be provided on upper floors or at the rear of the property.

Figure 2-2 Development Standards Summary Map includes a graphical representation of key development standards within the Specific Plan area.

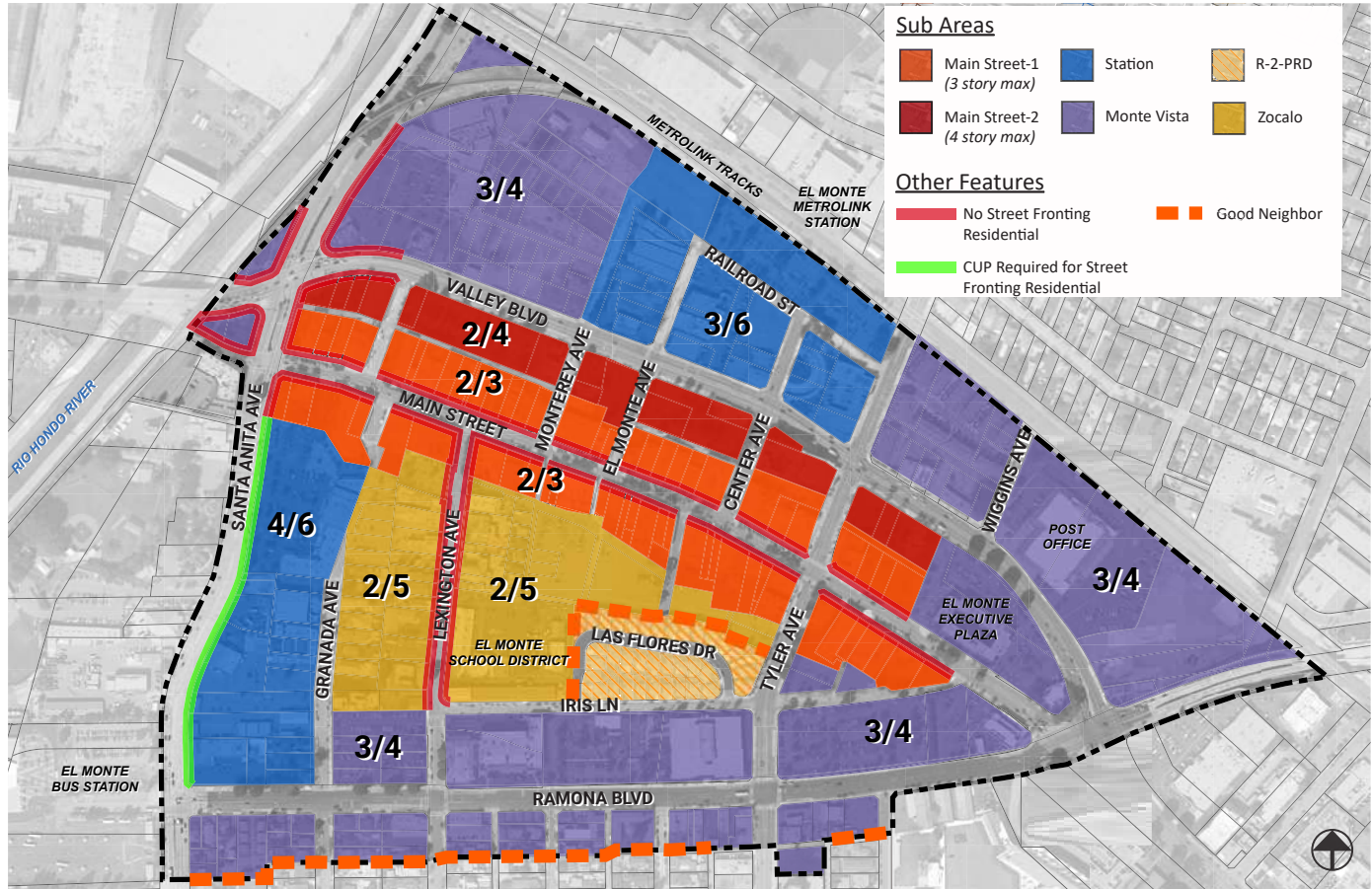


Figure 2-2 Development Standards Summary Map

TABLE 2-1 DEVELOPMENT OPPORTUNITY RESERVE¹

Regulation	By-Right	DOR - 1	DOR - 2
Main Street Subarea			
Max Height	30 ft.	40 ft.	50 ft.
Max Stories	2	3	4
Max FAR	1.0	1.5	2.0
Min Du/AC	--	25 du/ac	25 du/ac
Max Du/AC	25 du/ac	35 du/ac	45 du/ac
Zócalo Subarea			
Max Height	35 ft.	60 ft.	-
Max Stories	2	5	-
Max FAR	1.0	2.5	-
MIN Du/AC	25 du/ac	30 du/ac	-
Max Du/AC	30 du/ac	65 du/ac	-

Regulation	By-Right	DOR - 1
Station Subarea		
Max Height	40 ft.	75 ft.
Max Stories	3	6
Max FAR	1.5	3.0
Min Du/AC	30 du/ac	35 du/ac
Max Du/AC	35 du/ac	80 du/ac
Monte Vista Subarea		
Max Height	40 ft.	50 ft.
Max Stories	3	4
Max FAR	1.0	2.0
Min Du/AC	25 du/ac	30 du/ac
Max Du/AC	30 du/ac	50 du/ac

¹ Minimum density only applies to residential-only projects. Maximum Floor Area Ratio (FAR) only applies to non-residential square footage.

Figure 2-3 Development Setbacks Summary Map includes a graphical representation of setback standard requirements for buildings within the Specific Plan area. Refer to the development regulations located in **Sections 2.3 - 2.7** of this Chapter to determine exact setback requirements.

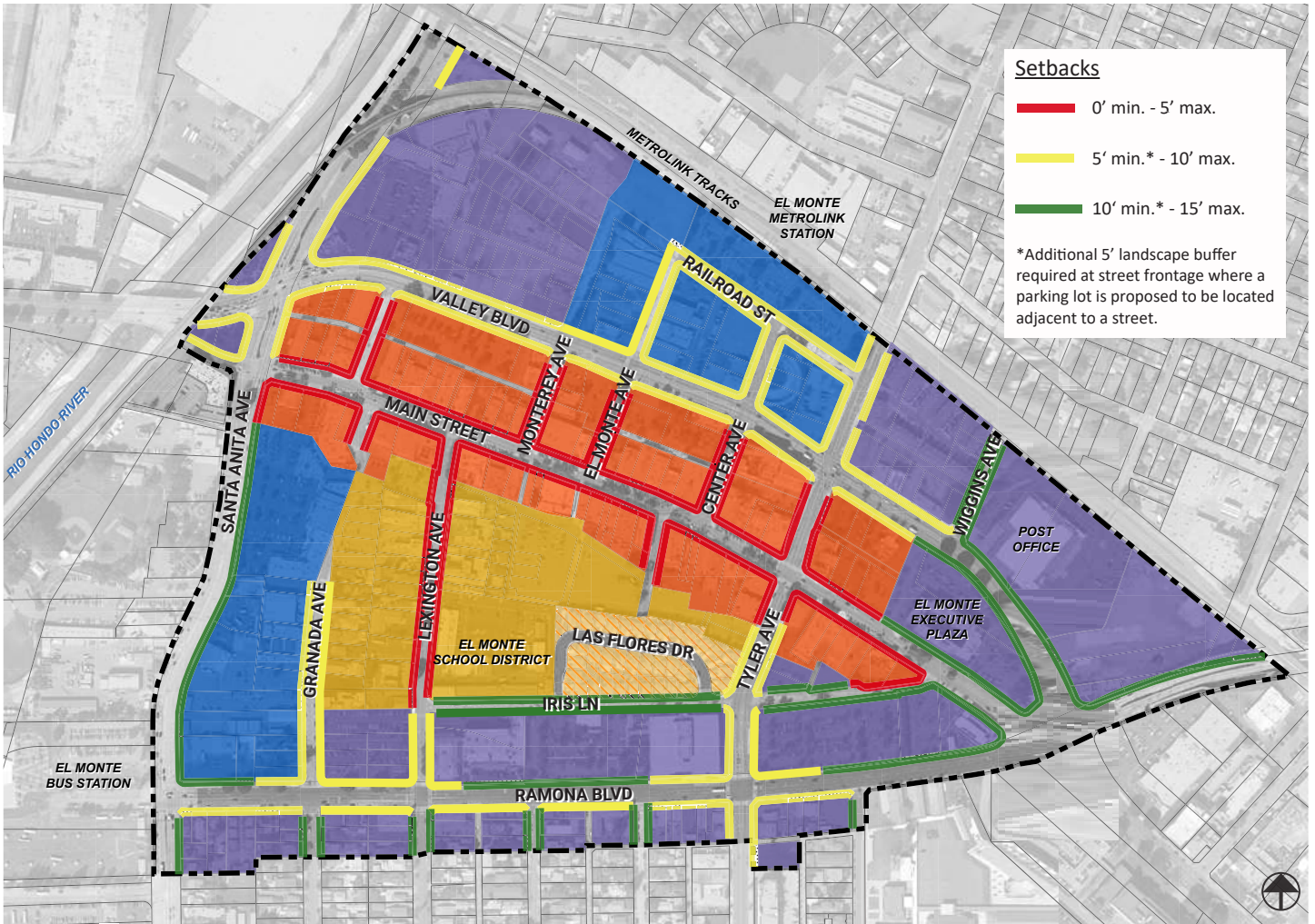


Figure 2-3 Development Setbacks Summary Map

2.2.2 DEVELOPMENT OPPORTUNITY RESERVE (DOR)

The intent of the Development Opportunity Reserve (DOR) is to encourage increased development intensity concurrently with the delivery of public improvements to satisfy the increased demand for public amenities that come with the increased development intensity.

Each sub-areas development standards identify the maximum height/stories, floor area ratio (FAR), and dwelling units per acre allowed by-right. However, a developer can exceed the development standards by-right in each sub-area through the DOR process. In order to exceed the by-right levels, the developer must provide certain on-site or off-site public amenities or pay an in-lieu fee. A summary of these amenities can be found in Table 2.2 (refer to Section 6.10 for further information on the list of amenities and the DOR process).

TABLE 2-2 POTENTIAL PUBLIC IMPROVEMENTS, PUBLIC AMENITIES, OR OTHER INCENTIVES

1. Provide affordable housing.
2. Construct square footage for the arts and other cultural uses.
3. Consolidate parcels of land.
4. Exceed the minimum parking requirements for the proposed land uses.
5. Provide publicly accessible parking spaces.
6. Incorporate on-site public art.
7. Provide publicly accessible open space. This can be for residential-only projects, mixed-use projects or nonresidential projects.
8. Construct public improvements (refer to Table 5-1).
9. Payment of an in-lieu fee.
10. Other publicly accessible improvements, amenities or other incentives considered by the Community and Economic Development Director.

2.2.3 PARKING MANAGEMENT STRATEGIES

In order to ensure an adequate amount of parking is provided within the Specific Plan area, a number of public and private parking management strategies have been identified below. These options represent a “toolbox”

of strategies that can be implemented based upon opportunities at the project level or at the Specific Plan area level. Many of the strategies will require coordination with property and/or business owners. While some of the strategies may be easier to implement in the short-term, other more long-term strategies will require additional effort and time to achieve.

Short-Term Parking and Employee Parking Plans

A number of street parking spaces can be restricted for short-term use during peak periods (30, 60, or 90 minute parking). This could especially be the case along Main Street and Lexington Avenue. This would benefit merchants, as the most convenient parking would be reserved for the convenience of customers. At the same time, an employee parking plan could be developed to require employees to park in designated zones.

Public Parking Fees

Public parking is currently free throughout Downtown, with no parking meters present. Integration of parking meters, especially on Main Street, could provide ongoing revenue for the City to assist in the construction of additional public parking facilities or structures.

Parking Structures

New conventional or automated parking structures will allow for Downtown expansion and redevelopment. Opportunity sites, identified in Section 5.3, may be appropriate to accommodate such facilities.

Public/Private Partnerships

The City could enter into partnerships with private developers of larger parcels to provide additional public parking in excess of the needs of an individual project.

Shared Parking

Shared parking is the use of a parking space to serve two or more individual land uses without conflict or encroachment. The ability to share parking spaces is the result of two (2) conditions: variations in the accumulation of vehicles by hour, by day, or by season at an individual land use; and relationships among the land uses that result in visiting multiple land uses on the same auto trip. Shared parking agreements present an opportunity to increase the available parking supply within Downtown where adjacent land uses complement one another, without having to construct new parking facilities.

The following shall be required to establish shared parking:

- Applications. Shared parking shall require the approval of a Minor Use Permit (MUP).
- Uses and properties. Shared parking may be available for two (2) or more uses on the same property or on separate properties. Properties may be separated a maximum 150 feet. If shared parking is for separate properties, a parking agreement shall be prepared and made part of the permanent official records of the Planning Division.
- Compliance with parking requirements. The parking requirements outlined in this Chapter may be reduced by up to 50 percent, subject to compliance with this Subsection. A Minor Variance shall not be permitted to further reduce the number of required parking spaces.
- Annual reporting. Verification of continued service shall be submitted to the Planning Division by January 1st of each year.

In-Lieu or Parking Credit Program

In-lieu or parking credit programs are when a new use pays a fee on a per space basis to meet parking requirements. Such fees are then pooled together to improve existing parking facilities or construct new parking facilities.

The following shall be required to secure parking credits:

- Applications. Parking credits shall require the approval of an Administrative Permit.
- Land uses. Parking credits may be available for residential and nonresidential uses permitted within the Downtown Main Street Specific Plan. Parking credits shall not be available for legal non-conforming uses.
- Pool of public spaces. Only public parking spaces shall be eligible for parking credits. This includes spaces within public parking lots and spaces along the street that are striped. A maximum subscription factor of two (2) shall apply towards the pool of public spaces (i.e. each public parking space shall equal two (2) parking credits). One (1) parking credit shall be equal to one (1) required parking space.
- Requests. The requested number of parking credits shall be available for public parking (i.e. available within the pool of public spaces). The maximum number of parking credits an applicant may receive is 50 credits. In addition, parking credits shall not exceed 50 percent of the parking requirements for the Monte Vista Subarea. Public parking spaces shall not be reserved for a specific use or property.
- Compliance with parking requirements. The use requesting the parking credits shall comply with the parking requirements outlined in this Chapter. This may be through a combination of on-site parking spaces, shared parking, and parking credits. The applicant shall not receive more parking credits than the use requires. A Minor Variance shall not be permitted to reduce the number of required parking spaces.
- Nontransferable. The parking credits shall not satisfy the parking requirements for any other use and shall not be transferred to another property.

- Written contract and payments. Parking credits shall require a contract between the applicant and the City of El Monte. Payments for the parking credits shall be due annually at the time of business license application/renewal.
- Time periods. Parking credits shall be valid for one (1) year. If the applicant has proceeded in good faith towards the implementation of the project, as determined by the Community and Economic Development Director, the applicant may request a 12-month extension. The extension shall be considered within 30 days of the request. No further extensions shall be given and the number of parking credits shall return to the pool of public parking. If the use has ceased for more than 12 months, the application shall be null and void and the parking credits shall return to the pool of public parking.

Additional Parking Strategies and Requirements

Another strategy available to reduce parking requirements for residential uses is car sharing. Parking spaces can also be reduced if the project includes affordable housing or senior housing.

Refer to Chapter 17.70 of the Zoning Ordinance for additional parking requirements and Chapters 17.72 and 17.74 of the Zoning Ordinance for landscaping requirements for parking lots.

2.2.4 PERMITTED AND CONDITIONALLY PERMITTED USES

An important component of this Specific Plan process is the provision of a mix of land uses that reflect the intended vision for each sub-area. Within this Chapter, the list of uses outlined in the Zoning Ordinance have been organized by sub-area to ensure consistency with the Specific Plan.

The uses shown in Table 2-3 as Permitted (P) are acceptable anywhere in the land use designation in which

they are located. They will not require special conditions when developed in accordance with this Specific Plan. Other uses are listed with an (A), which means they need the review and approval of an Administrative Permit by the Community and Economic Development Director. Refer to Chapter 17.120 of the Zoning Ordinance for additional information on the City’s ministerial process for certain new uses.

Other uses require some level of discretionary review. These uses either require a Minor Use Permit (M), which are approved by the City’s Zoning Review Committee or require a Conditional Use Permit (C), which are approved by the City’s Planning Commission. Refer to Chapters 17.121 and 17.123 of the Zoning Ordinance for additional information on the City’s discretionary review process.

Uses that are not permitted are indicated with a ‘-’. Uses not indicated in Table 2-3 are not permitted except as outlined in Section 17.12.050 of the Zoning Ordinance.

Construction of new building, additions to existing buildings and remodeling existing buildings are subject to design review. Refer to Chapter 17.122 of the Zoning Ordinance for additional information on the City’s design review process.

Definitions of uses are provided in Chapter 17.150 of the Zoning Ordinance.

TABLE 2-3 PERMITTED USES - DOWNTOWN SPECIFIC PLAN

	Main Street & Zócalo	Station ¹	Monte Vista	Zoning Code
Residential - Main and Ancillary Uses				
Accessory Building	--	--	P	17.110.020
Factory-built housing	--	P/C	P	
Home occupation	P	P	P	17.110.040
Live/work unit	--	M	M	17.110.050
Mixed-use development –				
Horizontal	--	P/C	P	17.110.060
Vertical ²	P	P	P	17.110.060
Multiple-family, five (5) or more attached dwellings	-/P ³	P/C	P	
Senior housing	C	P/C	P	
Urban housing	-/P ⁴	P/C	P	17.110.120
Community Care Uses				
Adult daycare facility, general	--	--	C	
Childcare facility	--	C	C	
Elder care or assisted living facility	--	--	C	
Residential care facility, general	--	--	C	
Public & Quasi-Public Uses				
Electrical distribution substation	--	--	C	
Government or government related facility [^]	P	P	P	
Recreation facility – public [^]	P	P	P	
School & Educational Facility -				
College or university, public	--	P	P	
College or university, private	--	C	C	
K-12, public	--	--	P	
K-12, private	--	--	C	
Preschool, public	--	P	P	
Preschool, private	--	C	C	
Specialized education and training	--	C	C	
Tutoring and education center*	P	P	P	
Urgent care medical center*	--	P	P	
Utility facility	--	--	C	
Wireless facility		See Notes		17.90 & 17.92
Assembly and Entertainment Uses				
Ancillary entertainment* [^]	P	P	P	
Assembly or meeting facility	C	C	C	
Commercial entertainment [^]	C	C	C	
Commercial recreation facility – indoor [^]	C	C	C	
Community center [^]	P	P	P	
Cultural institution [^]	C	C	C	
Family entertainment center	--	C	C	
Gaming center or arcade	--	C	C	
Nightclub [^]	C	C	C	
Religious institution	--	--	C	
Retail and Office Uses				
Alcohol sales⁵ –				
Assembly and entertainment use, on-site ⁶	C	C	C	17.112.030
Bar or tavern, on-site [^]	C	C	C	17.112.030
Brew pub, on-site and off-site	--	--	C	17.112.030
Liquor store, off-site	--	--	C	17.112.030
Restaurant, limited hours, on-site* [^]	M	M	M	17.112.030
Restaurant, on-site* [^]	M	C	C	17.112.030
Retail store, off-site* [^]	C	C	C	17.112.030
Cannabis activity, commercial –dispensary* [^]	P	P	P	
Convenience store or minimart* [^]	M	M	C	
Food or beverage establishment –				
Bakery or patisserie, retail* [^]	P	P	P	
Coffeehouse or café* [^]	P	P	P	
Outdoor seating* [^]	P	P	P	17.112.130
Restaurant* [^]	P	P	P	
Grocery store* [^]	P	P	P	
Multiple-tenant commercial center* [^]	P	M	M	17.112.120

	Main Street & Zócalo	Station ¹	Monte Vista	Zoning Code
Retail and Office Uses				
Offices –				
Ancillary* [^]	P	P	P	
Administrative, business professional*	P	P	P	
Government*	P	P	P	
Medical*	P	P	P	
Office supply store* [^]	P	P	P	
Pharmacy* [^]	P	P	P	
Retail sales (unless listed as a separate use)* [^]	P	P	P	
Secondhand vendor* [^]	M	M	M	17.112.170
Temporary use	--	--	T	17.124
Vehicle retail use –				
Parts and accessory store	--	--	P	
Sales and lease, limited	--	--	M	
Service Uses				
Animal service –				
Animal grooming [^]	P	P	P	
Veterinary service or animal hospital/clinic	--	--	P	
Automated Teller Machine (ATM), walk-up* [^]	P	P	P	17.112.050
Drive-through business –				
Food or beverage establishment	--	--	C	
Service or retail	C	C	M	
Financial institution*	P	P	P	
Funeral home or mortuary	--	--	P ⁷	
Hotels and motels* [^]	C	C	C	17.112.100
Office concierge service*	P	P	P	
Personal service use –				
General* [^]	P	P	P	17.112.150
Restricted* [^]	M	M	M	17.112.150
Philanthropic or charitable institution*	P	P	P	
Recycling facility –				
Mobile	--	--	P	17.112.160
Self-service	--	--	A	17.112.160
Wedding chapel	M	M	M	
Transportation Uses				
Transit station	--	P	P	
Vehicle parking –				
Attendant or valet parking	C	C	C	
Car sharing, residential	M	M	M	17.70.070(C)
Car sharing, nonresidential use ⁸	M	M	M	
Parking structure	M	M	M	
Shared parking	M	M	M ⁹	17.70.070(D)

* Use is pedestrian oriented and may occupy the ground floor of vertical mixed-use buildings. Other uses shall not be permitted on the ground floor.

[^] Use is pedestrian oriented and may occupy the ground floor of buildings facing Main Street west of Tyler Avenue.

-- Use not permitted.

P Use permitted by-right.

A Use permitted after review and approval of an Administrative Permit (AP).

T Use permitted after review and approval of a Temporary Use Permit (TUP).

M Use permitted after review and approval of a Minor Use Permit (MUP).

C Use permitted after review and approval of a Conditional Use Permit (CUP).

¹ Ground floor residential facing Santa Anita Avenue shall require approval of a CUP.

² For vertical mixed-use projects with residential, a minimum 50% of the total floor area shall be residential.

³ Only permitted in Main Street DOR-2 and Zócalo Subareas. Not permitted in Main Street DOR-1.

⁴ Only permitted in Main Street DOR-2 and Zócalo Subareas. Not permitted in Main Street DOR-1.

⁵ Alcohol sales can be for beer and wine or for beer, wine and distilled spirits. However, they shall be considered different levels of alcohol sales.

⁶ Only applicable to assembly and entertainment uses permitted in the underlying zoning district.

⁷ Funeral home or mortuary shall not include crematories.

⁸ Car sharing shall be permitted by-right if there is no on-site parking of vehicles (i.e. it is only an office use)

⁹ Parking structure shall be ancillary to a permitted use.

2.3 AREA WIDE DEVELOPMENT STANDARDS

The following development standards apply to all sub-areas within the Specific Plan area. Additional sub-area specific development standards are located in **Sections 2.4** through **2.7**.

2.3.1 SITE DEVELOPMENT STANDARDS

Site planning and design is key to quality development. The following standards promote orienting buildings toward the street, minimizing curbcuts, improving pedestrian connectivity and safety, and buffering single family residences from larger scale development.

1) Building Location

- a. Buildings shall be oriented toward the street. The minimum separation between buildings shall be 10'. (Figure 2-4).

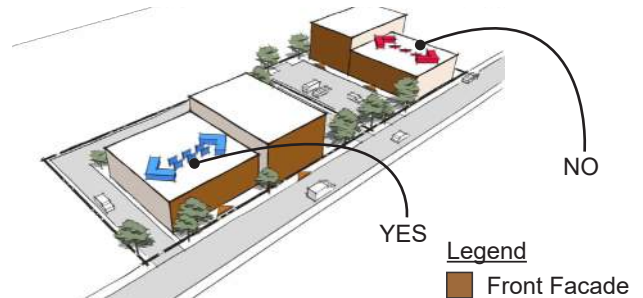


Figure 2-4 Building Orientation

2) Ingress and Egress

All Downtown Streets:

- a. Reciprocal ingress and egress with adjacent properties shall be provided for parcels fronting public streets. The City may waive this depending on the location of existing structures, infrastructure, or failure to reach an agreement between owners (Figure 2-5).

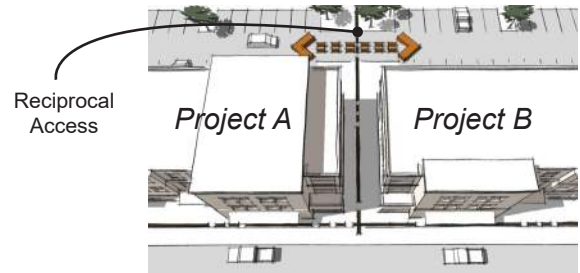


Figure 2-5 Ingress / Egress / Reciprocal Access

Main Street:

- a. Only access to a parking structure is permitted from Main Street. All other vehicular access shall be provided via an alley.

Valley Boulevard, Ramona Boulevard, and Santa Anita Avenue:

- a. Access shall be taken from alleys, where the condition exists.
- b. A maximum of one (1) vehicle access point shall be provided from a primary public street for parcels with less than 150 linear feet of street frontage and/or a parcel less than 15,000 sf (Figure 2-6).
- c. A maximum of two (2) vehicle access points shall be provided from a primary public street for parcels with more than 150 linear feet of street frontage (Figure 2-6).

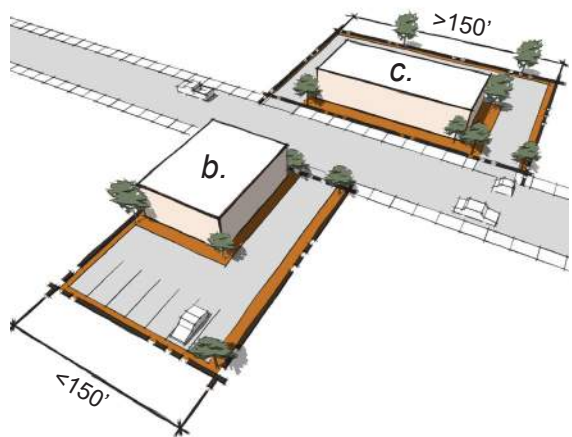


Figure 2-6 Single and Double Vehicle Access

AREA WIDE SITE DEVELOPMENT STANDARDS

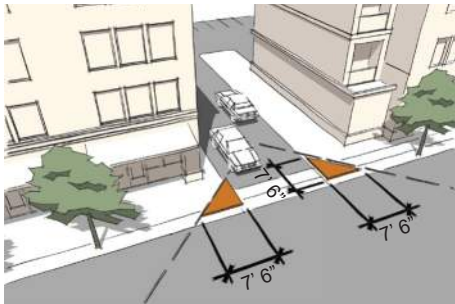


Figure 2-7 Vehicular Line of Sight

3) Vehicular Line of Sight

- a. The clear zone shall consist of an isosceles right triangle with 7'-6" sides from the edge of curb (Figure 2-7).
- b. The clear zone shall not be occupied by a site features or landscaping that is taller than 3'.

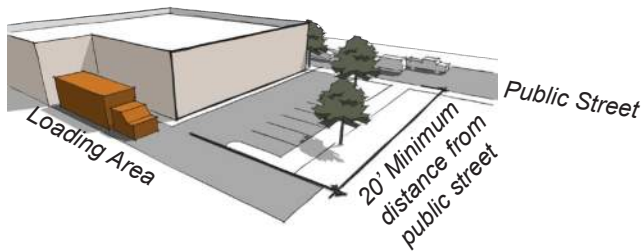


Figure 2-8 Service and Delivery Areas

4) Service and Delivery Areas

- a. Loading docks and service areas shall not be visible from Main Street and Lexington Avenue.
- b. For all other streets, loading docks and service areas shall be located a minimum of 20' from a public streets and be screened from view (Figure 2-8).
- c. Trash and recycling enclosures shall not be located adjacent to a public street and shall be located as far from on-site/off-site residential as possible.

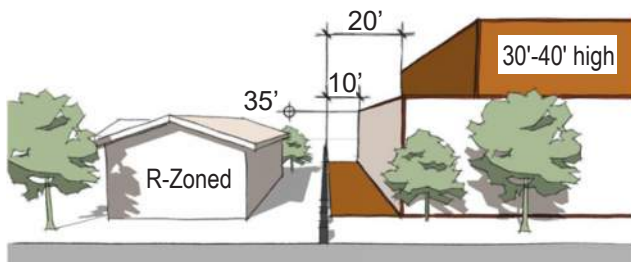


Figure 2-9 Privacy Buffer Setback for Portions of Structures over 35' in Height

5) Good Neighbor Buffer Development Standards

Good neighbor development standards are provided to ensure future development within the Specific Plan area is compatible with existing residential development located adjacent to projects greater than three stories and within proposed mixed-use projects.

Parcels Adjacent to Residential Zones

Privacy Buffer:

- a. Projects that are located adjacent to R-zoned properties shall be setback 10' minimum from the residential property line. Portions of a building between 30' and 40' tall shall be setback 20' minimum from the residential property line (Figure 2-9). Those portions of a structure over 40' tall shall be setback 30' minimum from the property line (Figure 2-10).

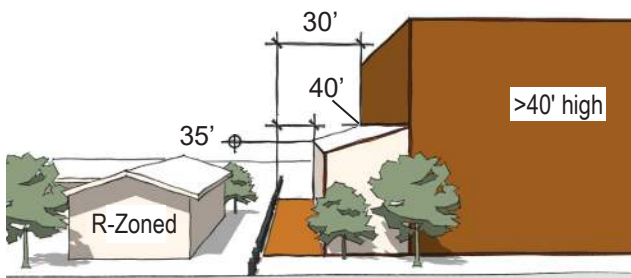


Figure 2-10 Privacy Buffer Setback for Portions of Structures over 40' in Height

AREA WIDE SITE DEVELOPMENT STANDARDS

5) Good Neighbor Buffer Development Standards

Continued Noise Buffer:

- a. Loading docks, service areas, and noise and odor-generating operations are not permitted within 20' of a property zoned R-1 and R2-PRD (Figure 2-11) or shall be buffered with a 5' landscaped setback and 8' masonry sound wall (Figure 2-12).
- b. HVAC systems shall be selected based on their noise rating or designed with features to reduce noise, such as parapet walls and equipment enclosures and/or placement of equipment.
- c. Noise-generating equipment, such as refrigeration units and air conditioning and exhaust fans shall be located away from residential uses. Noise-reducing screens and insulation may be required if any equipment has the potential to create a negative impact on residential uses.

Visual Buffer:

- a. All exterior lighting shall focus internally within the property to decrease light pollution onto neighboring residential properties. Outdoor lighting shall be shielded in a manner that prevents a direct line between its luminary and any property zoned R-1/R2-PRD. Pedestrian lighting at a minimum of one foot candle shall be required. Landscaping shall be used with other features to reduce potential visual, light, and glare conflicts between non-residential uses and residential uses.
- b. Landscaping shall be used with other features to reduce potential visual, light, and glare conflicts between non-residential uses and residential uses.

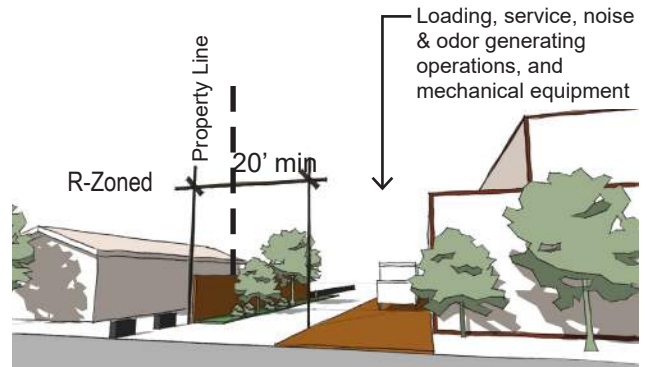


Figure 2-11 Noise Buffer

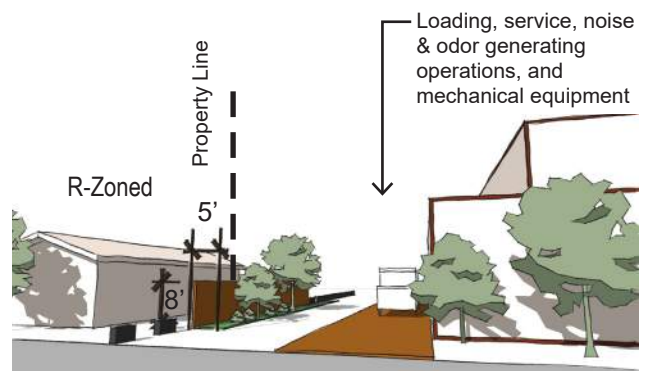


Figure 2-12 Noise Buffer

6) Building Design Conformance

- a. All projects shall conform with the intent of the El Monte Comprehensive Design Guidelines and design guidelines found in this Specific Plan.
- b. Mixed/multi-use projects shall comply with regulations found in Chapter 17.30 - Mixed/Multiuse Zone, of the City's Zoning Ordinance.

AREA WIDE SITE DEVELOPMENT STANDARDS**7) Public Open Space Requirements**

Proposed projects with more than 3,500 square feet of nonresidential space shall include a public open space amenity, or some form of physical interface for the pedestrian. The minimum size of the space shall be 10% of the combined nonresidential footprint. The space may be divided into subareas, provided each subarea is a minimum 350 sf. Such space may include, but not be limited to:

- a. *Formal plazas. A formal plaza would be a publicly accessible open space which has a design that is influenced by classical urban planning design. A formal plaza would typically include some sort of central water fountain and/or symmetrical landscaping. The formal plaza shall have a minimum dimension of 15 linear feet horizontally in each direction with an unobstructed vertical height of 7'.*
- b. *Urban gardens. An urban garden can be located on the ground level, or on upper levels of a structure. Urban gardens may include ornamental landscaping arranged in raised or at-grade planters or planting areas, potted plants and trees. Sculptures or other forms of public art would typically be included within the urban garden. The urban garden shall have a minimum dimension of 15 linear feet horizontally in each direction with an unobstructed vertical height of 7'.*
- c. *Covered colonnades. Colonnades are linear in design and generous in depth. The intent is to provide a comfortably wide, covered pathway that is adjacent to the openings of a building. Sometimes the second floor of a building is utilized to create the "covered" element of the colonnade.*
- d. *Sidewalk dining. Sidewalk dining may occur wherever a sidewalk space is ample enough to accommodate dining furniture without impeding pedestrian access of the sidewalk. Sidewalk dining may be defined with a railing or planters, or be open and accessible.*
- e. *Pedestrian alleys and walkways. A pedestrian alley or walkway is typically a "lane" that does not follow the alignment of a vehicular street, but provides a pedestrian access to a public space or some other feature within the interior of a development. It must be designed in such a manner as to be inviting to pedestrians. Therefore, issues such as lighting, security, line of sight, cleanliness and visual appeal are important considerations to a well-designed pedestrian alley or walkway. Public art, street furniture and access to shops and public spaces are features of pedestrian alleys and walkways.*

8) Facade Articulation (Street Fronting)

- a. *Buildings shall have a primary entrance door facing public streets. Entrances at building corners may be used to satisfy this requirement (Figure 2-13).*
- b. *A combination of architectural design elements shall be used on the building facade, along with streetscape elements at the street level, to animate and enliven the streetscape. These design elements may include but are not limited to: ornamentation, molding, changes in material or color, architectural lighting, works of art, fountains, display areas, awnings, balconies, porches, towers, landscaped planter boxes, trellises, columns, cornices, arches, decorative tiles, decorative grillwork, and outdoor furniture along street frontages.*



Figure 2-13 Building Entrances

AREA WIDE DEVELOPMENT STANDARDS

9) Facade Articulation (Non-Street Fronting)

- a. *Non-street fronting facades shall not exceed 50' without a change in wall plane, roof-line, and/or change in material (Figure 2-14).*

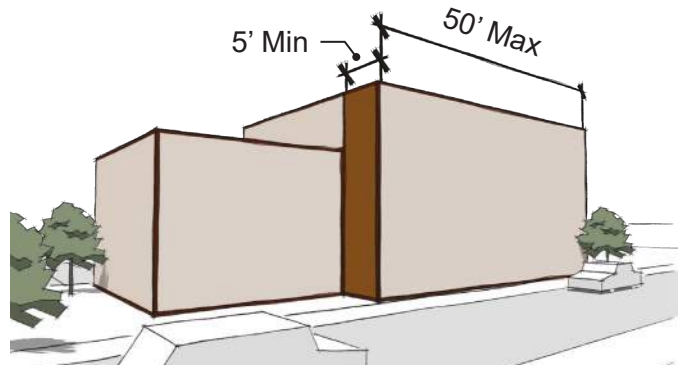


Figure 2-14 Building Facade Form

10) Security Screening

- a. *Security screening shall comply with Section 17.60.110 - Security bars and gates, of the City's Zoning Ordinance.*
-

2.3.2 AREA WIDE PARKING DEVELOPMENT STANDARDS

The following parking standards shall apply to the Specific Plan Area:

PARKING REQUIREMENTS FOR RESIDENTIAL USES		
DWELLING SIZE OR # OF BEDROOMS	DWELLING UNITS/ACRE*	
	≤35	>35
Studio	1.0	1.0
1 bedroom	1.5	1.0
1,500 sf or 2 bedrooms	2.0	1.5
2,000 sf or 3 bedrooms	2.0	2.0
>2,500 sf or ≥4 bedrooms	2.25	2.0
Guest parking for 3+ units	1 per 6 units	1 per 8 units
Minimum guest spaces	2	2

* Density shall be prior to any State density bonuses

PARKING REDUCTIONS FROM THE ZONING ORDINANCE		
NONRESIDENTIAL LAND USES	MS, Z, AND ST	MV
Assembly and entertainment uses	20%	None
Food and beverage establishments	30%	20%
Retail, office and service uses	40%	30%
All other uses	As required in the Zoning Ordinance	

Projects shall comply with Chapter 17.70 – Parking Regulations, of the Zoning Ordinance. This includes compliance on items such as access and safety, drainage, lighting, loading, paving materials, ramp/driveway slope, stall dimensions, striping, visibility, bicycle parking and electric charging stations. Applicants should also refer to their applicable project sub-area (Section 2.4 through Section 2.7) for additional, specific parking standard requirements.

2.4 MAIN STREET SUB-AREA



Figure 2-15 Main Street Sub-Area

2.4.1 SUB-AREA CHARACTER

The Main Street Sub-Area includes the area identified in *Figure 2-15*. Main Street is the commercial heart of Downtown El Monte with many charming one- and two-story mom-and-pop stores. Window displays, wide sidewalks, accent landscaping, and street trees make Main Street a pleasant place to walk. As surrounding sub-areas begin to develop with additional mixed-use development and multi-family housing, it is anticipated that the demand for day-to-night uses will increase (e.g. restaurants, entertainment) along Main Street, thereby enhancing its vibrancy.

The plaza envisioned for this Sub-Area will act as a hub for community gathering, cultural arts events, and performance venues for residents and visitors.

This Sub-Area also includes parcels on the south side of Valley Boulevard between Santa Anita Avenue and Wiggins Avenue where an opportunity occurs for redevelopment at a greater height and intensity. Many of these parcels are surface parking lots with the potential to transform into multi-family and mixed-use developments with retail on the first floor and residential above.

As the Main Street Sub-Area redevelops, it will be important to retain the existing essence of Downtown El Monte that is most characterized by alley and paseo connections, architectural variety, pedestrian scaled buildings, and walkable streets.

MAIN STREET SUB-AREA

2.4.2 MAIN STREET DEVELOPMENT STANDARDS

The following development standards are specific to the Main Street Sub-Area and have been tailored to reflect the desired character and vision as set forth in **Section 2.4.1** and Chapter 1.

1) Building Use

a. See Table 2-3 for permitted uses.

2) Lot Coverage

a. 100% maximum.

3) Floor Area Ratio (FAR) (Figure 2-2)

For non-residential uses.

- a. 1.0 maximum.
- b. 1.5 with DOR along Main Street.
- c. 2.0 with DOR along Valley Boulevard.

4) Residential Density (Figure 2-2)

- a. 25 units per acre maximum.
- b. 35 units per acre maximum with DOR along Main Street.
- c. 45 units per acre maximum with DOR along Valley Boulevard.

5) Building Height (Figures 2-2, 2-17)

- a. 30' maximum (2 stories).
- b. 40' maximum with DOR along Main Street (3 stories).
- c. 50' maximum with DOR along Valley Boulevard (4 stories).
- d. Commercial ground floor plate heights shall be a minimum of 12' (Figure 2-18).
- e. Residential ground floor plate heights shall be a minimum of 9'.

6) Open Space (Figure 2-19)

The minimum amount of open space required for each residential unit shall be as follows (prior to any density bonus):

Overall Open Space

- a. ≤35 units per acre: 160 sf per unit (vertical mixed use). 200 sf per unit (all other projects).
- b. Between 35 and 45 units per acre: (vertical mixed use). 175 sf per unit (all other projects).



Figure 2-16 Building Use

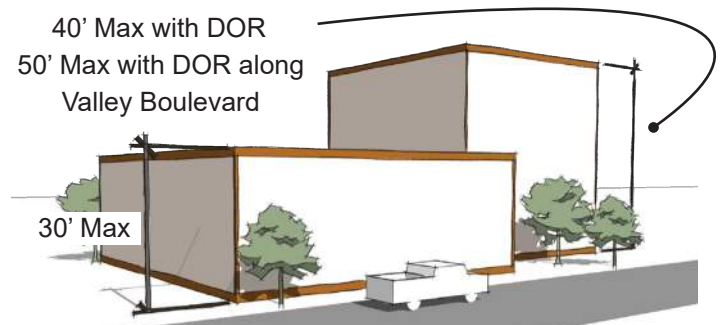


Figure 2-17 Building Height



Figure 2-18 Commercial Ground Floor Plate Height

MAIN STREET SUB-AREA

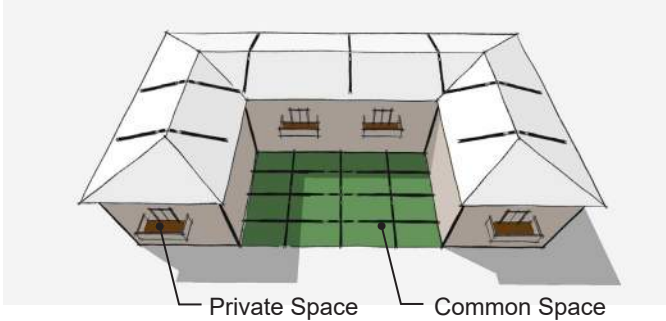


Figure 2-19 Open Space Requirements

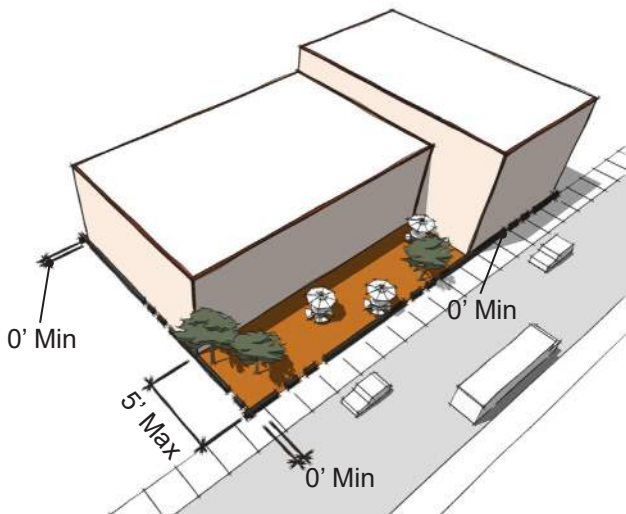


Figure 2-20 Street Setbacks (Main Street)

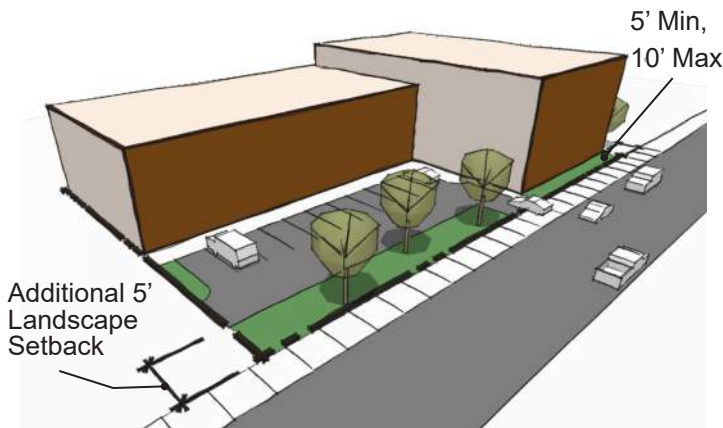


Figure 2-21 Parking Lot Located Adjacent to Street

Private Open Space

- a. ≤ 35 units per acre: No minimum (vertical mixed use). 60 sf per unit (all other projects).
- b. Between 35 and 45 units per acre: No minimum (vertical mixed use). 40 sf per unit (all other projects).
- c. Unobstructed vertical clearance: 7'.
- d. Minimum dimensions: 6' in any direction (4' for balconies).

Common Open Space

- a. ≤ 35 units per acre: 40 sf per unit and minimum 500 sf for any common open space area (all projects).
- b. Between 35 and 45 units per acre: 50 sf per unit and a minimum of 400 sf for any common open space area (all projects).
- c. Unobstructed vertical clearance: 7'.
- d. Minimum dimensions: 12'.
- e. Indoor space: 30% or 600 sf, whichever is greater (for projects ≤ 35 units per acre). 35% or 800 sf, whichever is greater (for projects > 35 units per acre).

SETBACKS

The intent is to create a consistent building edge at the back of the sidewalk, allowing minor variations in setbacks to create spaces or pockets for outdoor seating areas, outdoor dining, and shopping.

7) Street Setbacks

- a. See Figure 2-3 and Figure 2-20.
- b. An additional 5' landscape buffer is required along the street frontage where a parking lot is proposed adjacent to a street and shall be landscaped per Section 2.3.2.g (Figure 2-21).

8) Interior Side and Rear Setback (Figure 2-20)

- a. 0' minimum.

9) Alley Setback (Figure 2-22)

- a. 8' minimum improved pedestrian access and landscaping shall be provided adjacent to a facade fronting an alley.

MAIN STREET SUB-AREA

10) Outdoor Dining (Figure 2-23)

- a. Shall comply with 17.112.130 - Outdoor Dining, of the City's Zoning Ordinance.

11) Main Street Facades (Figures 2-24, 2-25)

The following requirements are intended to create building forms that are pedestrian friendly and are representative of El Monte's historical character.

- a. Buildings shall provide a primary entrance and façade on Main Street.
- b. 50% minimum transparent glazing on first floor façade for retail uses adjacent to a public street. Opaque or reflective glazing is not permitted.
- c. Along Main Street and Lexington Avenue, buildings greater than 40' of linear street frontage shall be designed to replicate a traditional 20'-40' storefront facade with a minimum 6" variation in wall plane.
- d. Building roof-lines shall provide variations in height a maximum distance of 40' in wall plane length.
- e. Parcels backing or siding onto paseos, parks, or plazas shall be designed with building articulation requirements consistent with Main Street facade requirements.

12) Parking Standards - Refer to Section 2.3.2

- a. New parking lots are not permitted adjacent to Main Street.

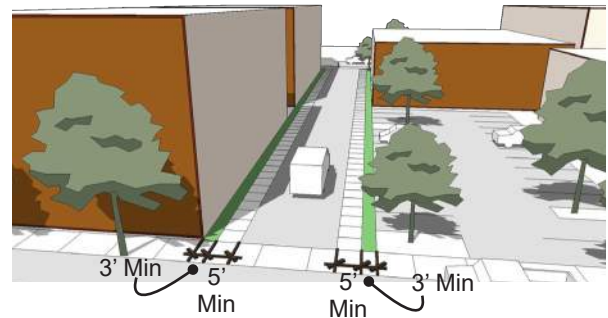


Figure 2-22 Alley Setback



Figure 2-23 Outdoor Dining



Figure 2-24 Building Facade Form Variation



Figure 2-25 Building Roof Form

2.5 ZÓCALO SUB-AREA



Figure 2-26 Zócalo Sub-Area

2.5.1 SUB-AREA CHARACTER

El Monte’s Zócalo Sub-Area encompasses the area shown on *Figure 2-26*. The term ‘Zócalo’ means public square or plaza and in order to live up to its name, a unique central plaza/green will become a key component of the Zócalo Sub-Area. This central plaza/green is envisioned as a community oriented, family-friendly destination providing the space and medium necessary to host local arts and cultural programs including art in the plaza, markets, musical performances, and other events.

Large surface parking areas located behind Main Street businesses provide an opportunity for artist live/work units, mixed-use projects, cultural spaces, parking garages, and/or multi-family residential structures.

Lexington Avenue between Main Street and Iris Lane is also included in this Sub-Area with an opportunity to serve as an extension of Main Street’s charm and character. Development opportunity is limited due to existing parcel sizes however lot consolidation may occur allowing for two- to three-story mixed-use buildings. Pedestrian circulation will be improved through the provision of enhanced paseo connections and with streetscape beautification treatments similar to Main Street.

ZÓCALO SUB-AREA

2.5.2 ZÓCALO SUB-AREA DEVELOPMENT STANDARDS

The following development standards are specific to the Zócalo Sub-Area and have been tailored to reflect the desired character and vision described in **Section 2.5.1** and Chapter 1.

1) Building Use

- a. See Table 2-3 for permitted uses.

2) Lot coverage (Figure 2-2)

- a. 100% maximum.

3) Floor Area Ratio (FAR) (Figure 2-2)

For non-residential uses.

- a. 1.0 maximum.
- b. 2.5 with DOR.

4) Residential Density (Figure 2-2)

- a. 25 units per acre minimum and 30 dwelling units per acre maximum.
- b. 65 units per acre maximum with DOR.

5) Building Height (Figures 2-26, 2-28)

- a. ≤35' maximum (2 stories).
- b. 60' maximum (5 stories) with DOR.
- c. Commercial ground floor plate heights shall be a minimum of 12'. Residential ground floor plate heights shall be a minimum of 9'.

6) Open Space (Figure 2-29)

The minimum amount of open space required for each residential unit shall be as follows (prior to any density bonus):

Overall Open Space

- a. ≤35 units per acre: 160 sf per unit (vertical mixed use). 200 sf per unit (all other projects).
- b. Between 35 and 50 units per acre: 140 sf per unit (vertical mixed use). 175 sf per unit (all other projects).
- c. >50 units per acre: 120 sf per unit (vertical mixed use). 150 sf per unit (all other projects).

Private Open Space

- a. ≤35 units per acre: No minimum (vertical mixed use). 60 sf per unit (all other projects).
- b. Between 35 and 50 units per acre: No minimum (vertical mixed use). 40 sf per unit (all other projects).
- c. >50 units per acre: No minimum (all projects).
- d. Unobstructed vertical clearance: 7'.
- e. Minimum dimensions: 6' in any direction (4' for balconies).

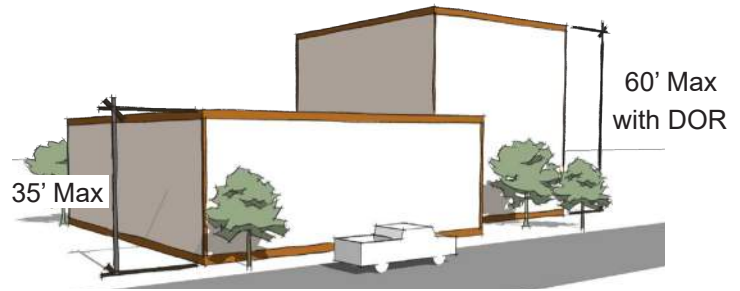


Figure 2-27 Building Height



Figure 2-28 Commercial Ground Floor Plate Height

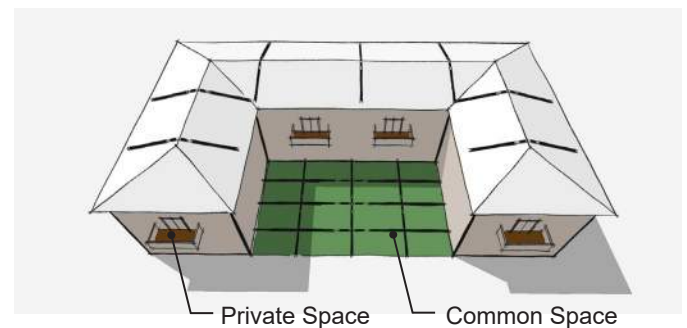


Figure 2-29 Open Space Requirements

ZÓCALO SUB-AREA

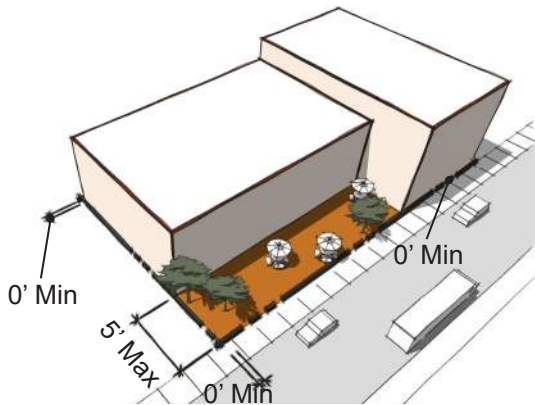


Figure 2-30 Street Setbacks (Lexington Avenue)

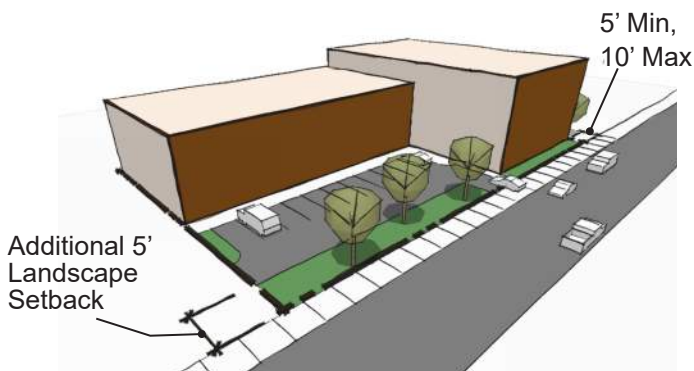


Figure 2-31 Parking Lots Located Adjacent to Street



Figure 2-32 Building Façade Form Variation



Figure 2-33 Building Roof Form

Common Open Space

- a. ≤ 35 units per acre: 40 sf per unit and minimum 500 sf for any common open space area (all projects).
- b. Between 35 and 50 units per acre: 50 sf per unit and a minimum of 400 sf for any common open space area (all projects).
- c. > 50 units per acre: 60 sf per unit and a minimum of 300 sf for any common open space area (all projects).
- d. Unobstructed vertical clearance: 7'.
- e. Minimum dimensions: 12'.
- f. Indoor space: 30% or 600 sf, whichever is greater (for projects ≤ 35 units per acre). 35% or 800 sf, whichever is greater (for projects > 35 units per acre).

SETBACKS

The intent is to create a consistent building edge at the back of the sidewalk, allowing minor variations in setbacks to create spaces or pockets for outdoor seating areas, outdoor dining, and shopping.

7) Street Setbacks

- a. See Figure 2-3 and Figure 2-30.
- b. An additional 5' landscape buffer is required along the street frontage where a parking lot is proposed adjacent to a street and shall be landscaped per **Section 2.3.2.g** (Figure 2-31).

8) Interior Side and Rear Setback (Figure 2-30)

- a. 0' minimum.

9) Lexington Avenue Façade (Figure 2-32, 2-33)

The following requirements are intended to create building forms that are pedestrian friendly and are representative of El Monte's historical character.

- a. Buildings shall provide a primary entrance and façade on Lexington Avenue.
- b. 50% minimum transparent glazing on first floor façade for retail uses adjacent to a public street. Opaque or reflective glazing is not permitted.
- c. Along Lexington Avenue, buildings greater than 40' of linear street frontage shall be designed to replicate a traditional 20'-40' storefront facade with a minimum 6" variation in wall plane.
- d. Building roof-lines shall provide variations in height a maximum distance of 40' in wall plane length.
- e. Parcels backing or siding onto paseos, parks, or plazas shall be designed with building articulation requirements consistent with Lexington Avenue facade requirements.

10) Parking Standards - Refer to Section 2.3.2

2.6 STATION SUB-AREA

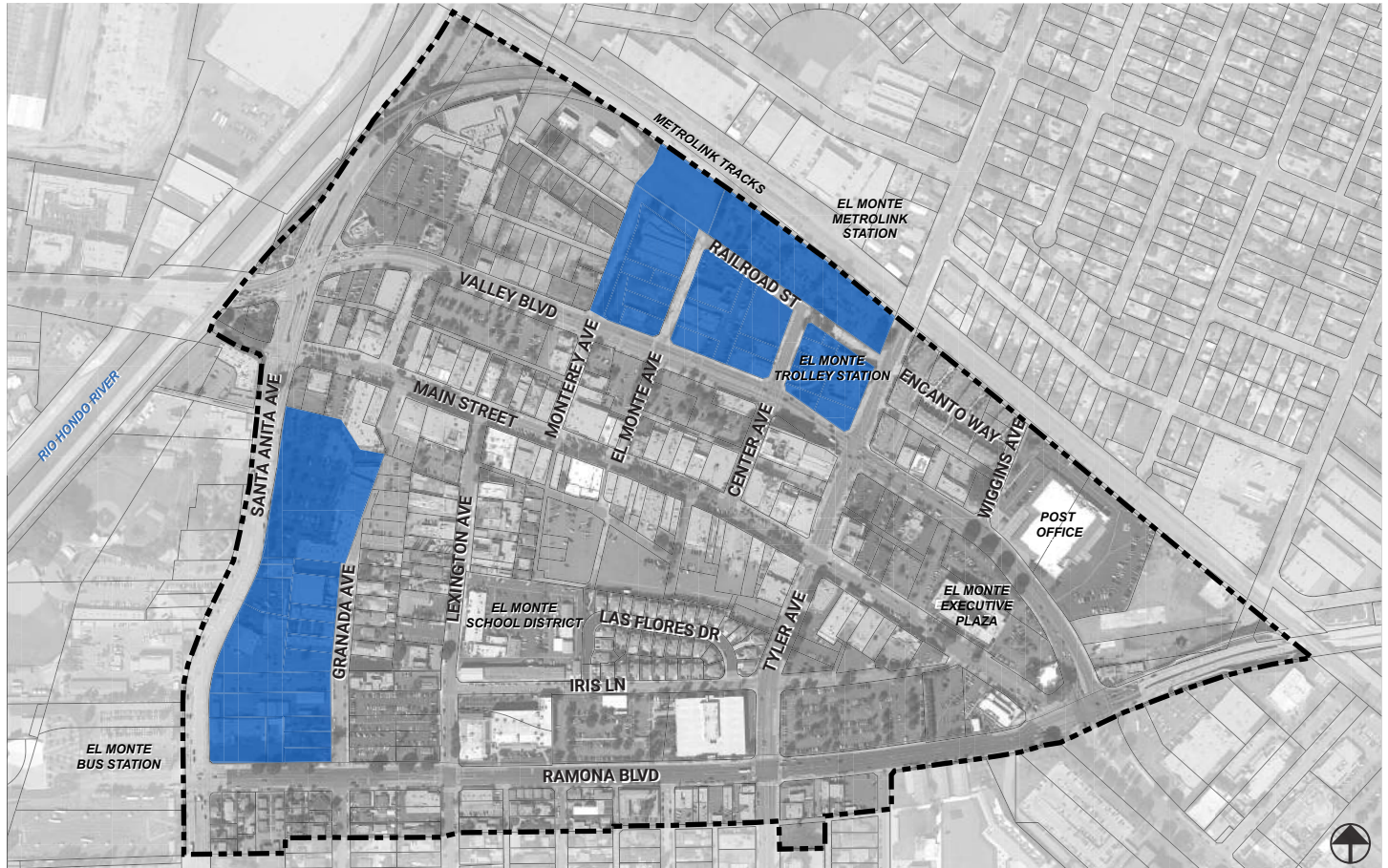


Figure 2-34 Station Sub-Area

2.6.1 SUB-AREA CHARACTER

The Station Sub-Area is divided into two areas within Downtown El Monte that are located near or adjacent to transit facilities and are intended to be redeveloped with transit-oriented development. The northern-most portion is located adjacent to a Metrolink Station. This northern area includes the opportunity to provide a dynamic mix of uses such as multi-family residential and visitor serving retail that are intended to serve commuters using the Metrolink, as well as nearby residents. Within the Station Sub-Area, there are multiple transit options to make local and regional connections without using an automobile. To support the transit-oriented culture, small complementing retail/mixed-use establishments will be carefully placed

to be best utilized by residents and visitors. Strong pedestrian-oriented connections and wayfinding improvements will be provided to guide pedestrians to the existing Santa Fe Trail Plaza, transit facilities, and Main Street.

The southern portion also serves as an entryway into Downtown El Monte and is directly adjacent to the El Monte Bus Station. Located along Santa Anita Avenue, the area currently is a mix of retail uses with some vacant lots. Development of this area will complement the El Monte Gateway Project that consists of transit serving multi-family residential and retail.

STATION SUB-AREA

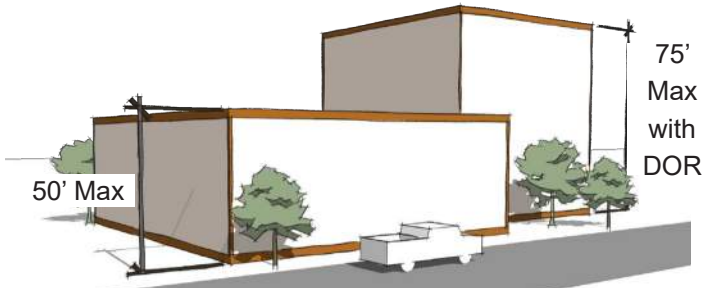


Figure 2-35 Building Height



Figure 2-36 Commercial Ground Floor Plate Height

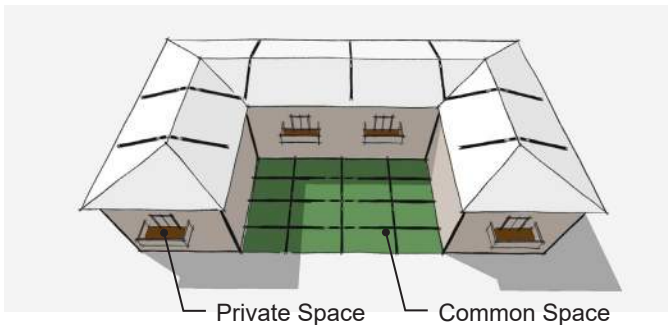


Figure 2-37 Open Space Requirements

2.6.2 STATION SUB-AREA DEVELOPMENT STANDARDS

The following development standards are specific to the Station Sub-Area and have been tailored to reflect the desired character and vision set forth in **Section 2.6.1** and Chapter 1.

- 1) Building Use**
 - a. See Table 2-3 for permitted uses.
- 2) Lot coverage (Figure 2-2)**
 - a. 90% maximum.
- 3) Floor Area Ratio (FAR) (Figure 2-2)**

For non-residential uses.

 - a. 1.5 maximum.
 - b. 3.0 maximum with DOR.
- 4) Residential Density (Figure 2-2)**
 - a. 30 units per acre minimum.
 - b. 35 units per acre maximum.
 - c. 80 units per acre maximum with DOR.
- 5) Building Height (Figures 2-2, 2-35, 2-36)**
 - a. 40' maximum (3 stories).
 - b. 75' maximum (6 stories) with DOR.
 - c. Commercial ground floor plate heights shall be a minimum of 12'. Residential ground floor plate heights shall be a minimum of 9'.
- 6) Open Space (Figure 2-37)**

The minimum amount of open space required for each residential unit shall be as follows (prior to any density bonus):

Overall Open Space

- a. ≤35 units per acre: 160 sf per unit (vertical mixed use). 200 sf per unit (all other projects).
- b. Between 35 and 50 units per acre: 140 sf per unit (vertical mixed use). 175 sf per unit (all other projects).
- c. >50 units per acre: 120 sf per unit (vertical mixed use). 150 sf per unit (all other projects).

Private Open Space

- a. ≤35 units per acre: No minimum (vertical mixed use). 60 sf per unit (all other projects).
- b. Between 35 and 50 units per acre: No minimum (vertical mixed use). 40 sf per unit (all other projects).

STATION SUB-AREA

- c. Unobstructed vertical clearance: 7'.
- d. Minimum dimensions: 6' in any direction (4' for balconies).

Common Open Space

- a. ≤35 units per acre: 40 sf per unit and minimum 500 sf for any common open space area (all projects).
- b. Between 35 and 50 units per acre: 50 sf per unit and a minimum of 400 sf for any common open space area (all projects).
- c. >50 units per acre: 60 sf per unit and a minimum of 300 sf for any common open space area (all projects).
- d. Unobstructed vertical clearance: 7'.
- e. Minimum dimensions: 12'.
- f. Indoor space: 30% or 600 sf, whichever is greater (for projects ≤35 units per acre). 35% or 800 sf, whichever is greater (for projects >35 units per acre).

8) 0' minimum.

9) 30% minimum transparent glazing of first floor facades adjacent to a public street.

- a. See Figure 2-3 and Figure 2-4.
- b. An additional 5' landscape buffer is required along the street frontage where a parking lot is proposed adjacent to a street and shall be landscaped per Section 2.3.2.g.
- a. 0' minimum.
- a. 30% minimum transparent glazing of first floor facades adjacent to a public street.
- b. No building facade shall extend more than 100' in length without a 5' minimum variation in the wall plane.
- c. Building roof-lines shall provide variations in height a maximum distance of 100' in wall plane length.

10) 100' maximum.

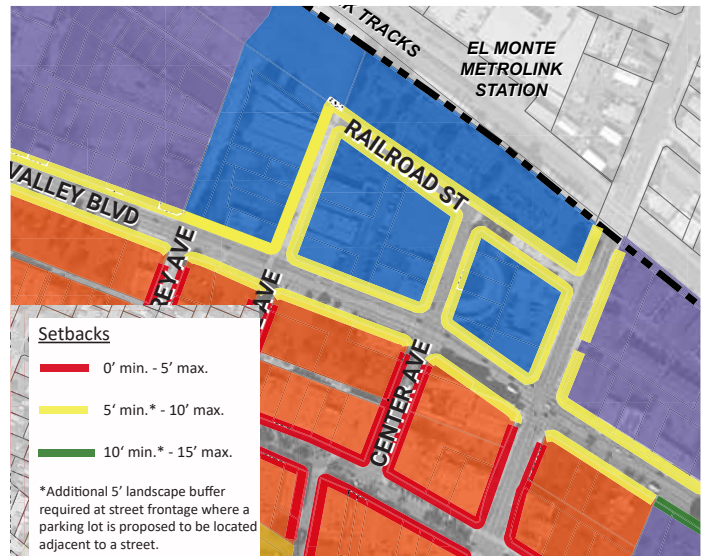


Figure 2-38 Development Summary Setbacks Map

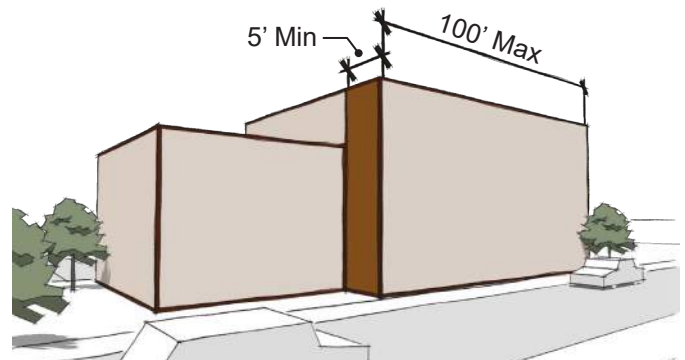


Figure 2-39 Building Facade Form



Figure 2-40 Building Roof Form

2.7 MONTE VISTA SUB-AREA

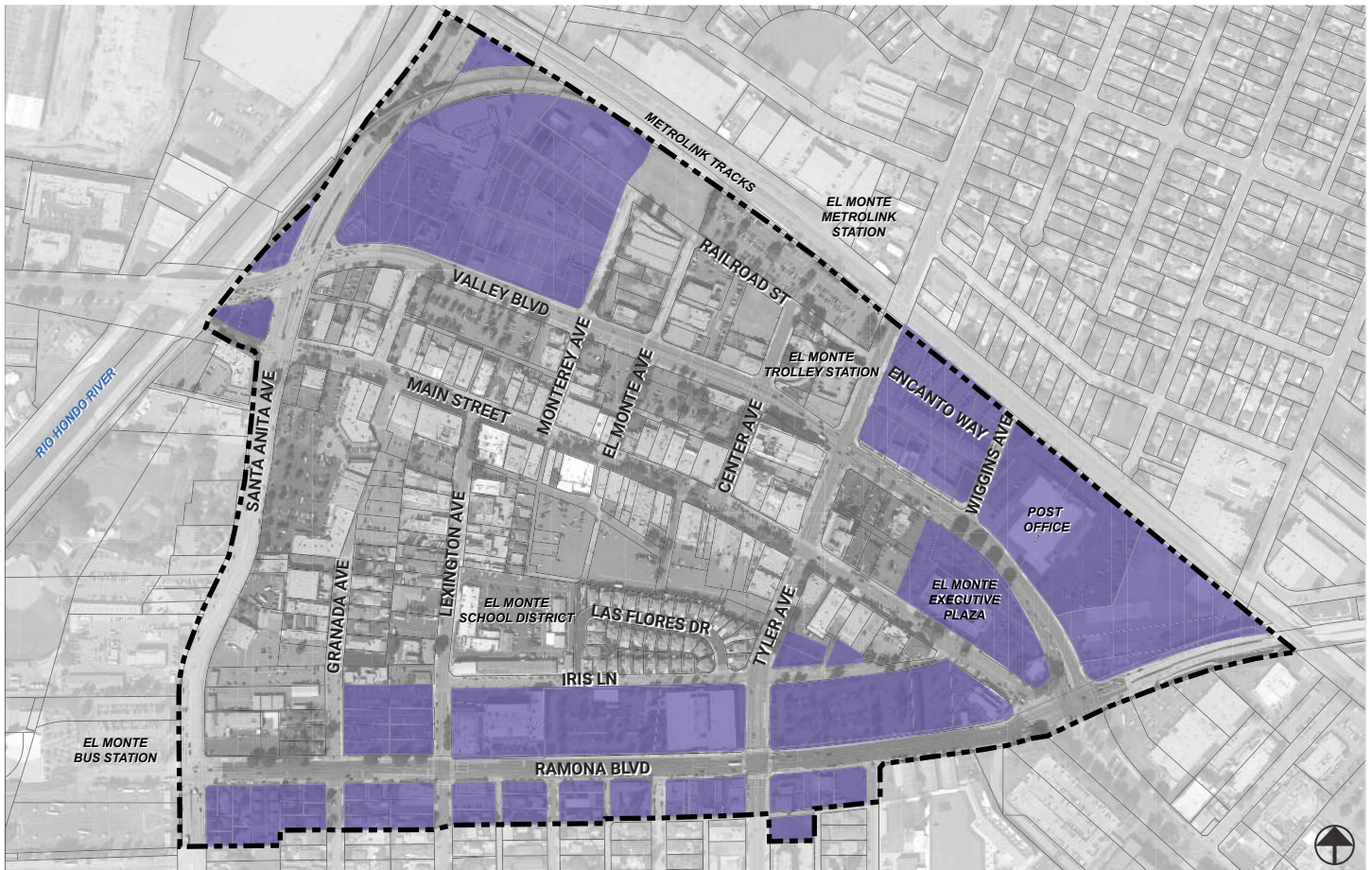


Figure 2-41 Monte Vista Sub-Area

2.7.1 SUB-AREA CHARACTER

The Monte Vista Sub-Area is a transition area between Downtown El Monte and surrounding neighborhoods. This Sub-Area is characterized by larger parcel sizes, destination retail, office, and public uses.

The Monte Vista Sub-Area is located along the southern and northern edges of the Specific Plan area and is comprised of office, residential and public uses, including the El Monte United States Post Office. The Sub-Area contains supporting uses and creates a buffer to surrounding residential uses.

The sub-area will be enhanced with street corridor beautification, mixed-use development, and Downtown entry treatments in key location. New development will be limited to a maximum of four stories, to ensure an appropriate transition from the Downtown core to areas outside the Specific Plan boundary.

MONTE VISTA SUB-AREA

2.7.2 MONTE VISTA SUB-AREA DEVELOPMENT STANDARDS

The following development standards are specific to the Monte Vista Sub-Area and have been tailored to reflect the desired character and vision as identified in **GYWjcb' &"+'%"** and Chapter 1.

2.7.2.1 Lot Coverage

a. See Table 2-3 for permitted uses.

2.7.2.2 Building Height

a. 80% maximum lot coverage.

2.7.2.3 Density

For non-residential uses.

a. 1.0 maximum.

b. 2.0 maximum with DOR.

2.7.2.4 Building Height

a. 25 units per acre minimum.

a. 30 units per acre maximum.

b. 50 units per acre maximum with DOR.

2.7.2.5 Building Height

a. 40' maximum (3 stories).

b. 50' maximum (4 stories) with DOR.

c. Commercial ground floor plate heights shall be a minimum of 12'. Residential ground floor plate heights shall be a minimum of 9'.

2.7.2.6 Open Space

The minimum amount of open space required for each residential unit shall be as follows (prior to any density bonus):

2.7.2.6.1 Density

≤35 units per acre: 160 sf per unit (vertical mixed use). 200 sf per unit (all other projects).

Between 35 and 50 units per acre: 140 sf per unit (vertical mixed use). 175 sf per unit (all other projects).

>50 units per acre: 120 sf per unit (vertical mixed use). 150 sf per unit (all other projects).

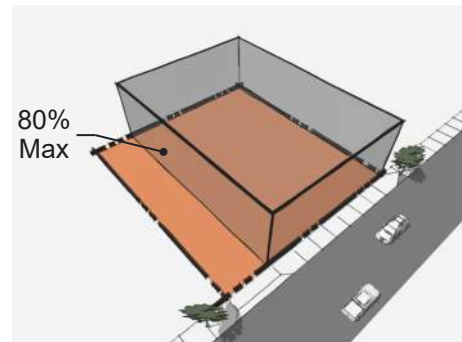


Figure 2-42 Lot Coverage

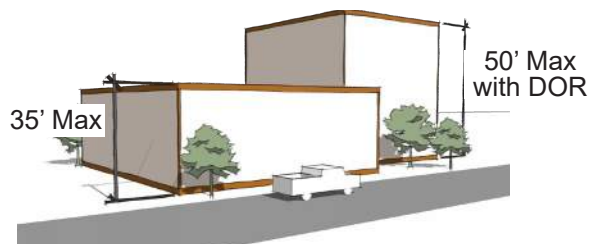


Figure 2-43 Building Height



Figure 2-44 Commercial Ground Floor Plate Height

MONTE VISTA SUB-AREA

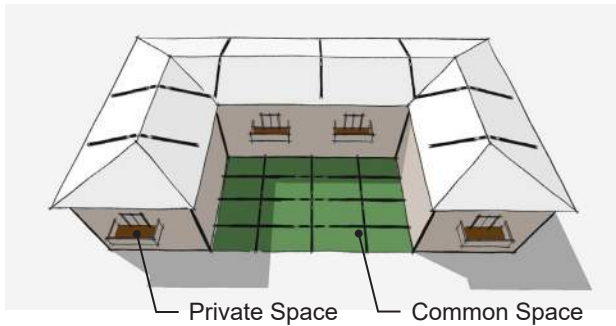


Figure 2-45 Open Space Requirements

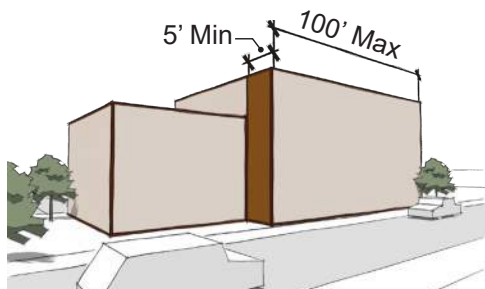


Figure 2-47 Building Facade Form



Figure 2-48 Building Roof Form

Private Open Space

- a. ≤ 35 units per acre: No minimum (vertical mixed use). 60 sf per unit (all other projects).
- b. Between 35 and 50 units per acre: No minimum (vertical mixed use). 40 sf per unit (all other projects).
- c. Unobstructed vertical clearance: 7'.
- d. Minimum dimensions: 6' in any direction (4' for balconies).

Common Open Space

- a. ≤ 35 units per acre: 40 sf per unit and minimum 500 sf for any common open space area (all projects).
- b. Between 35 and 50 units per acre: 50 sf per unit and a minimum of 400 sf for any common open space area (all projects).
- c. Unobstructed vertical clearance: 7'.
- d. Minimum dimensions: 12'.
- e. Indoor space: 30% or 600 sf, whichever is greater (for projects ≤ 35 units per acre). 35% or 800 sf, whichever is greater (for projects > 35 units per acre).

SETBACKS

7) Street Setbacks

- a. See Figure 2-3.
- b. An additional 5' landscape buffer is required along the street frontage where a parking lot is proposed adjacent to a street and shall be landscaped per **Section 2.3.2.g**.

8) Interior Side and Rear Setbacks

- a. Side - 5' minimum to building.
- b. Rear - 10' minimum to building.

MONTE VISTA SUB-AREA**9) Building Form (2-47, 2-48)**

- a. *30% minimum transparent glazing of first floor façade adjacent to a public street.*
- b. *No building façade shall extend more than 100' in length without a 5' minimum variation in the wall plane.*
- c. *Building roof-lines shall provide variations in height a maximum distance of 100' in wall plane length.*

10) Parking Standards - Refer to Section 2.3.2

2.8 AREA WIDE DESIGN GUIDELINES

The following guidelines are provided as a supplement to the El Monte Comprehensive Design Guidelines found on the City’s website. **All projects shall be designed to comply with Chapter 17.140 – Comprehensive Design Guidelines, of the City’s Zoning Ordinance.** Duplication has been avoided, however, should a conflict occur, the guidelines provided herein shall be utilized. The guidelines have been organized to provide direction on sustainability, site design, building form and massing, articulation, and utilitarian aspects of a project and should be adhered to irrespective of use.

The following Section contains both standards and guidelines. It is important to make a distinction between the two types of regulations. Standards are mandatory regulations that must be satisfied by all development to which the standards apply. In addition to the standards, future development is also informed by design guidelines which are not mandatory requirements, but provide a defined framework of the design principles that supplement the standards.



Reuse of existing historic structure.

2.8.1 HISTORIC COMPATIBILITY

When a new development or rehabilitation of an existing is adjacent to or in the immediate area of an historic property, the development or rehabilitation shall comply with Secretary of Interior’s Standards and Guidelines for the Treatment of Historic Properties. In addition, it shall be consistent with the following design guidelines for historic compatibility.

- a. *Maintain historic features and elements of existing buildings.*
- b. *Restore existing historic features that may have been altered from past remodels (e.g. remove a false façade on a building to reveal its original brick exterior).*
- c. *New construction should respect and complement the original period and style of adjacent buildings without mimicking them exactly.*
- d. *New construction should avoid a false “historical” look.*



Blend of historic and contemporary architecture.

AREA WIDE DESIGN GUIDELINES**2.8.2 SUSTAINABILITY**

Sustainability includes the protection and conservation of irreplaceable non-renewable resources. The transit oriented nature of this Specific Plan is inherently sustainable as the intent is to connect nearby residential neighborhoods to transit facilities, services, shopping, and dining establishments through a series of walkable streets and paseos. This will lead to a reduction in auto dependency, encourage an active lifestyle, and add a sense of vibrancy to Main Street.

Sustainability is a common theme found throughout the City's "Vision El Monte" 2011 General Plan including the following two goals and policy:

GOAL CD-4: *High-quality architectural design of residential, commercial, and industrial buildings evidenced by thoughtful attention and balance of quality materials, durability, aesthetics, functionality, and sustainability concepts.*

CD-4.5: *Sustainability. Encourage "green building" and environmentally sustainable design concepts with respect to energy conservation, water conservation, storm drainage, etc.*

GOAL LU-4: A complementary balance of land uses that provide adequate opportunities for housing, economic activity, transportation, parks, and recreation to support an exemplary quality of life and a sustainable community.

As a sustainability leader in the nation, California has mandated that all new residential and government construction shall be Net Zero by the year 2020 and all new non-residential construction shall be Net Zero by 2020-2025 and commercial construction shall be Net Zero by 2030. Net Zero is achieved when the amount of energy provided by on-site renewable energy sources is equal to the amount of energy used by the building. This will be achieved through continued revision to the California Green Building standards. The latest version of the California Green Building Standards Code (Title 24, Part 11) or "Cal Green" that went into effect January 2019, is intended to lead building owners, designers and builders in that direction. Cal Green currently includes

both mandatory and voluntary provisions. Further, the El Monte Municipal Code includes Title 14 – Sustainability that addresses water conservation and tree protection/preservation regulations. One of the City's General Plan objectives is to expand Title 14 to include additional sustainable regulations. Applicants, designers, and developers must be familiar with the latest Cal Green and the El Monte Municipal Code Title 14 standards when starting a project. In addition, the following design guidelines relating to sustainability are strongly encouraged for incorporation into development projects within the Specific Plan area. However, the provisions and requirements of Cal Green shall supersede the design guidelines where there is a conflict.

1. General Sustainable Design Guidelines

- a. *Residences should be within one-quarter mile or less from services, transit, and other daily needs, and should have strong pedestrian connections.*
- b. *Auto-dependency should be reduced by providing pedestrian linkages through walkways or bike paths to encourage accessibility to residential and service needs.*
- c. *Public parks and plazas should provide well-linked active and passive open space for an opportunity for exercise and recreation.*
- d. *Buildings should be designed to take advantage of solar orientation to ensure that windows, walls, and floors are built to collect, store, and distribute solar heat efficiently.*
- e. *Buildings should be oriented to maximize active and/or passive solar gain; this allows the facades to let light in, reduce glare, and reduce overheating to the building interior.*
- f. *Architectural elements such as skylights and high-performance glazing should also be used to conserve energy, where possible.*

AREA WIDE DESIGN GUIDELINES

- g. Climatic factors such as prevailing winds, shade trees, window and door orientation, and the positioning of buildings should be coordinated to maximize energy conservation.*

2. Stormwater Management Design Guidelines

- a. The project site should be designed to maintain natural stormwater flows by promoting infiltration.*
- b. Impervious surfaces should be minimized.*
- c. Site drainage should be designed to integrate a decentralized system that distributes stormwater across a project site.*



Pervious pavement.



Green roof.

- d. Various devices that filter water and infiltrate water into the ground should be considered.*
- e. For areas with poor drainage conditions, the provision of drainage chimneys/wells, subsurface water storage, or the provision of bioswales to clean the water and transfer off-site should be provided.*
- f. Practices to control pollution, such as phased construction, seeding, non-invasive grading, mulching, filter socks, stabilized site entrances, sediment controls, fiber rolls, and stabilized channels and outlets, are recommended.*

3. Existing Building Reuse Design Guidelines

- a. The reuse of existing structures is recommended. The energy required to reuse a structure is potentially less than the energy required to develop a new structure.*
- b. Building reuse should reduce the material waste that results from destroying old sites and rebuilding using new materials.*
- c. Reused buildings should incorporate new architectural elements in juxtaposition with old ones where appropriate.*
- d. For historic buildings, energy efficiency and sustainable preservation resources and strategies are available at: <http://www.nps.gov/tps/sustainability/energy-efficiency.html>.*

4. Cool Roof and Green Roof Design Guidelines

- a. Both cool roofs and/or green roofs should be considered on buildings.*

5. Solar Voltaic System Design Guidelines

- a. New residential and non-residential buildings are recommended to have solar-ready roofs.*
- b. Solar panels are recommended on rooftops and parking area carports.*

AREA WIDE DESIGN GUIDELINES

2.8.3 SITE DESIGN

The following guidelines are designed to enhance the overall site layout to achieve the pedestrian-oriented vision for the Specific Plan area.

- a. *Buildings should be oriented toward the street and/or publicly accessible space such as a plaza.*
- b. *Landscaping should be installed between the street and the sidewalk and/or trees planted in tree wells to buffer the sidewalk from traffic and to provide an enhanced pedestrian area.*
- c. *Parking lots should be provided behind buildings, underground, or within parking structures, where possible.*
- d. *Outdoor spaces should have a clear purpose that reflects careful planning and not simply “left over” areas between structures. Such spaces should be landscaped and/or provide pedestrian amenities, including benches, bicycle racks, fountains, and/or public art and should be coordinate with the City as part of a development proposal.*
- e. *Focal points should be developed at intersections, corners, and at the end of streets and pedestrian walkways to create a sense of identification. Plazas, landscape, fountains, artwork, textured pavement, and vertical building features may be combined to create focal points.*
- f. *Communal open spaces (e.g. plazas, common green spaces) should be integrated within commercial areas and residential developments to provide places for occupants and visitors to relax, play, and interact.*
- g. *Loading areas and delivery service areas at the rear or side of buildings should be screened with decorative walls, trellises/vines, berms with landscaping, trees, or a combination of these treatments.*
- h. *Parking areas should be landscaped to minimize glare and heat buildup and to reduce negative visual impact associated with large paving areas.*
- i. *Landscaping within parking areas should be protected from encroaching vehicles by concrete curbing or raised planting areas.*



Communal open space.



Outdoor spaces reflecting intentional planning.



Intersection focal point.

AREA WIDE DESIGN GUIDELINES



Central communal open space.



Commercial developments along paseos.



Pedestrian linkage between buildings.

- j. *The primary entry drive to parking lots, as well as pedestrian paths of travel within a parking lot, should be accented with enhanced paving and landscaping. Permeable or semi-permeable surfaces such as pavers are preferred for accent paving.*
- k. *Dead-end drive aisles should be avoided.*

2.8.4 PEDESTRIAN AND VEHICULAR CONNECTIONS

The circulation pattern throughout the Specific Plan area will play a pivotal role in the success of attracting visitors and potential tenants to the Downtown. It is important to establish a well-connected system of streets and paths, both internally and to surrounding uses, to allow users to choose from a variety of transportation modes including public transit, walking, and biking.

- a. *Plazas and outdoor use areas should be enhanced with shade trees or shade structures and pedestrian amenities such as benches, fountains, landscaping, and public art.*
- b. *Pedestrian paths and paseos should be designed as integral circulation routes to foster a vibrant environment that encourages walking and enhances the pedestrian experience.*
- c. *Access between transit/bus stops and Main Street should be clearly defined.*
- d. *Surfaces made from permeable materials such as pavers should be provided, where possible.*
- e. *Trellises and other pedestrian-scale amenities are encouraged in and along pedestrian paths.*
- f. *Outdoor furniture and fixtures should be compatible with the project architecture and should be carefully considered as integral elements of the project.*

2.8.5 OUTDOOR DINING

The following guidelines have been included to enhance and encourage outdoor dining within the Specific Plan area.

AREA WIDE DESIGN GUIDELINES

- a. *A minimum six (6) foot unobstructed clearance should be maintained from any object, fixture, or edge of curb to allow for pedestrian access and passage.*
- b. *All tables and chairs should be of sturdy construction, made of quality materials, and designed to complement the character of the attached use. The maximum diameter of the tables should be 48”.*
- c. *Parking for outdoor dining is limited to a maximum 10% of the required spaces. No additional parking is required for outdoor dining in excess of the 10% identified above.*
- d. *Refer to Section 17.112.130 of the Zoning Ordinance for additional standards related to outdoor dining.*
- e. *Other conditions as determined by the Community and Economic Development Director.*



Outdoor dining enhances the public realm.

2.8.6 BUILDING FORM AND MASS

Design building forms to create interesting architecture that relates to pedestrian scale, creates a Downtown character, and minimizes the appearance of large box-like buildings.

- a. *Variation in wall and roof planes should be used on large monolithic structures to break up the boxlike appearance. Surface detailing, such as score lines, should not serve as a substitute for distinctive massing.*
- b. *Building designs should incorporate 360-degree architecture. 360-degree architecture is full articulation on every building elevation. This includes variation in massing, roof forms, and wall planes, as well as surface articulation.*
- c. *Architectural elements that add visual interest, scale, and character such as projecting balconies, trellises, recessed windows, window detailing, and door detailing should be incorporated to create shadow patterns and help articulate façades and blank walls.*



Variation in building massing.



Color and materials used to define building base and anchor building.

AREA WIDE DESIGN GUIDELINES



Building form represents a series of different building facades and character.



Roof variation aides in breaking up building massing.



Deep roof overhang with exposed structural elements.

- d. *Building surfaces that face walkways should be effectively articulated to enhance the pedestrian experience.*
- e. *Contrasting base materials and/or molding elements should be used to anchor the building to the ground plane.*
- f. *Entries, display windows, awnings, arcades, and outdoor eating areas should be provided to create inviting public spaces.*
- g. *The visual impact of large monolithic structures should be minimized by creating a cluster of smaller buildings or the appearance of a series of smaller buildings.*
- h. *Stairways should be designed as an integral part of the overall architecture of the building and not appear as tacked on.*

2.8.7 ROOF FORM

Roof forms should be used to distinguish various building forms, create an interesting roof-line, and help to break up the building massing.

- a. *Buildings with flat or low pitched roofs should incorporate parapets or architectural elements to break up long horizontal rooflines.*
- b. *Deep roof overhangs are encouraged to create shadow and add depth to façades. Exposed structural elements (beams, rafter tails) are encouraged as roof overhang details.*
- c. *Full roofs are desirable. Hipped or gable roofs covering the entire building are preferred to mansard roofs and segments of pitched roofs applied at the building edge.*
- d. *Roof parapets should be well-detailed, be three dimensional, and of substantial size to complement the building. They should include one or more of the following detail treatments: pre-cast elements, continuous banding or projecting cornices, dentils, caps, corner details, or variety in pitch (sculpted).*

AREA WIDE DESIGN GUIDELINES

- e. *Parapets should be designed to avoid visibility of the interior. If the interior side of a parapet is visible from pedestrian view, it should be finished with the same materials and a similar level of detail as the front façade.*
- f. *Parapets should not appear “tacked on” and should convey a sense of permanence.*



Windows articulated with proportional shutters.

2.8.8 WINDOWS, DOORS, AND ENTRIES

- a. *Window, doors, and entry design and materials should complement the desired architectural style of the building.*
- b. *Entry design should incorporate two or more of the following methods:*

- *Change in wall/window plane;*
- *A projecting element above the entrance;*
- *A change in material or detailing;*
- *Architectural elements and decorative fixtures;*
- *A portico or formal porch projecting from or set into the surface;*
- *Changes in the roofline or a tower;*
- *Front porch; and*
- *Decorative detailing or placement of art.*



Prominent building entry.

- c. *Where recessed entries occur, a decorative paving material, such as tile, marble, or slate, is encouraged on the ground plane.*
- d. *Windows should be articulated with accent trim, sills, shutters, window flower boxes, balconies, awnings, or trellises authentic to the architectural style of the building.*
- e. *Windows should be inset from building walls to create shade and shadow detail.*
- f. *Faux shutters should be proportionate to the windows to create the appearance of a real and functional shutter.*
- g. *Long, monotonous balconies and exterior corridors that provide access to multiple units should be avoided. Instead, access points should be clustered.*



Common awning design and location.

AREA WIDE DESIGN GUIDELINES



Awnings located over doors and windows.



Authentic and timeless materials.



Contrasting colors to accent details such as trim and windows.

2.8.9 AWNINGS

- a. *All lots with more than one commercial occupant sharing contiguous frontage should maintain a common location and design for building awnings.*
- b. *The bottom edge of awnings should be a minimum of eight feet off the ground.*
- c. *All awnings should be constructed with noncombustible materials such as a fireproof canvas or vinyl.*
- d. *Awnings should be centered over doorways and windows.*
- e. *Awnings should be in proportion to door and window openings.*
- f. *Awnings should be of a color which is appropriate to the overall colors of the facade.*
- g. *The shape, size, and number of awnings should be appropriate to the overall facade design and building size.*
- h. *Awning structure should be sufficient to support safely the awning and withstand the pressures of wind and weather.*
- i. *Awnings which become ripped, tattered, or damaged must be repaired or replaced within three (3) months of the occurrence/appearance of the damage.*

2.8.10 MATERIALS AND COLORS

- a. *To convey a sense of high-quality design and permanence, building colors and materials should be selected to simulate authentic and timeless materials.*
- b. *Contrasting colors are encouraged to accentuate details such as trim, windows, doors, and key architectural elements, as long as selected colors reinforce the desired architecture style.*
- c. *Fluorescent paints and bright colors are strongly discouraged.*

AREA WIDE DESIGN GUIDELINES

- d. *Building base materials should be selected that are durable and highly resistant to pedestrian traffic such as pre-cast concrete, stone, masonry, brick, and commercial grade ceramic tile.*
- e. *Recommended façade materials:*
 - *Exterior plaster (smooth troweled preferred)*
 - *Cut stone, rusticated block (cast stone), stone tile, and pre-cast concrete*
 - *Brick veneer, new or re-used*
 - *Ceramic tiles*
- f. *Façade materials that are discouraged:*
 - *Imitation and crushed rock*
 - *Mirrored or reflective glass and heavily tinted glass*
 - *Windows with “tape on” divisions/mullions*
 - *Corrugated fiberglass*
 - *Vinyl and aluminum siding*
 - *Painted or baked enamel metal awnings*
 - *Rough “Spanish lace” stucco finish*
 - *Exposed concrete masonry units and split faced concrete masonry.*
 - *Imitation brick*
 - *Plywood siding*
- g. *Recommended roof materials:*
 - *Roof tiles made of clay, slate, or integrally colored concrete*
 - *Roof tiles with “Mission” or “Barrel” shaped roof profiles*
 - *Metal roof panels with standing seam texture*
 - *Ridge and hip caps and/or flashing should coordinate with field colors*
- h. *Roof materials that are discouraged:*
 - *Brightly colored material*
 - *Low-profile composition roof tile, wood and/or hard board, or synthetic shingles and shakes*
 - *Simulated clay tile roofs in metal*
 - *Corrugated metal roof panels*
 - *Roof tiles with S-profile*



Variety of material and colors in a mixed-use building.

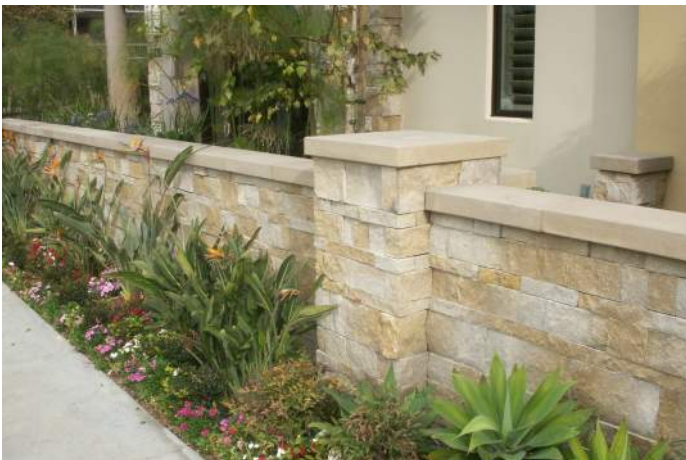


A variety of materials on a residential facade.



Low walls designed to complement building materials and design.

AREA WIDE DESIGN GUIDELINES



A combination of low walls and landscaping used for screening and to establish private yard areas.



Compatible light fixtures.



Exterior lighting illuminating signage.

2.8.11 WALLS AND FENCES / SCREENING

Minimize impact of walls along public streets.

- a. *Walls and fences should be designed with materials and finishes that complement project architecture and should be planted with vines, shrubs, and/or trees.*
- b. *All fences and walls required for screening purposes should be of solid material. Chain link or similar metal wire fencing with slats is prohibited for screening purposes.*
- c. *A combination of low walls and landscaping should be used to screen unsightly elements of the project and define private and semi-private areas.*
- d. *Fences and walls should be constructed as low as possible while still performing screening, noise attenuation, and security functions.*
- e. *Service areas are to be separate and screened from public areas by the use of walls and landscaping as much as possible.*
- f. *Refer to Section 17.60.130 of the Zoning Ordinance for additional standards related to walls, fences and screening.*

2.8.12 LIGHTING

Provide sufficient lighting for the safety of site occupants and visitors.

- a. *Light fixtures should be designed to relate in color, material, size, location, and illumination with the building architecture.*
- b. *All building entrances should be well-lit.*
- c. *Exterior building lighting designated to illuminate signs should be mounted above the sign on the facade and should be appropriate to the size and scale of the signage.*
- d. *Pedestrian and auto-oriented street lighting (light posts) will be provided by the City. No private pedestrian street lighting posts are permitted.*

AREA WIDE DESIGN GUIDELINES

- e. *Pedestrian alley lighting (light posts) will be provided by the City. No privately installed pedestrian alley lighting posts are permitted.*
- f. *Alley lighting, marking building entries, should be surface mounted and may be either flush with, recessed, or extended from the wall. No fluorescent lighting is allowed for this purpose.*
- g. *Fluorescent lighting tubes shall be concealed by a shade or lens.*
- h. *Parking lots, pedestrian walkways, and paseos should be illuminated to ensure safe nighttime conditions.*
- i. *Light fixtures should be sited, directed, and/or shielded to prevent spot lighting, glare, or light spillage beyond property lines.*
- j. *The lighting of building elements and trees is an effective and attractive lighting technique that is encouraged; however, light sources for wall washing and tree lighting should be hidden and located so as to not shine in the eyes of pedestrians.*
- k. *Low-voltage / high efficiency lighting should be used in the landscape.*
- l. *No floodlights are permitted.*
- m. *Security lighting fixtures should not project above the fascia or roofline of the building and should not be substituted for parking lot or walkway lighting fixtures.*
- n. *Timers and sensors should be incorporated to avoid unnecessary lighting in low volume pedestrian traffic areas.*
- o. *Refer to Section 17.60.050 of the Zoning Ordinance for additional standards on outdoor lighting.*



Entry lighting.



Self-clinging vines on trash enclosure.



Trash enclosure with materials similar to primary building.

2.8.13 TRASH ENCLOSURES

Carefully design, locate, and integrate trash enclosures into the site plan.

- a. *Enclosures should be located away from adjacent residential uses to minimize nuisances to neighboring properties.*
- b. *Self-clinging vines are encouraged to discourage graffiti.*

AREA WIDE DESIGN GUIDELINES



Landscaping screening mechanical equipment.

- c. Enclosures should be separated from adjacent parking stalls with a planter and paved surface behind the curb to provide easy access to a vehicle as well as adequate screening.
- d. Trash enclosures should be designed with similar finishes, materials, and details as the primary building(s) within the project area.
- e. Chain link or similar metal wire fencing with slats is prohibited.
- f. Enclosures should be unobtrusive and conveniently located for trash disposal by tenants and collection by service vehicles.
- g. Where possible, a pedestrian entrance to the trash enclosure should be provided so that the large access doors do not have to be opened as frequently.
- h. Refer to Section 17.60.080 of the Zoning Ordinance for additional standards on trash enclosures.

2.8.14 MECHANICAL EQUIPMENT SCREENING

- a. Mechanical equipment on the ground should be completely screened from street level view with attractive planting, masonry walls, or iron fencing with plantings which compliment building architecture.
- b. All roof-mounted mechanical equipment should be located behind a permanent parapet wall and completely screened from ground level view.
- c. Refer to Section 17.60.100 of the Zoning Ordinance for additional standards on screening.

2.8.15 SECURITY GATES

- a. Refer to Section 17.60.110 of the Zoning Ordinance for standards on security gates.

2.8.16 PARKING STRUCTURES

Parking structures that seamlessly integrate with surrounding buildings are appropriate for Downtown El Monte. This can be accomplished by wrapping portions of the first floor with commercial uses and by articulating upper floors with elements that reflect an occupied building.

AREA WIDE DESIGN GUIDELINES

- a. *Decorative and interesting architectural elements, such as towers and rotundas, should be utilized at street intersections. These elements could be used for stairwells and/or elevator towers.*
- b. *Parapets should be added to key areas on the structure to change the roof-line and reduce its horizontal appearance.*
- c. *Substantial massing should occur at the corner of the structure to anchor the building and give the structure proportions similar to a regular commercial building.*
- d. *Horizontal openings should be broken up with vertical columns to create a rhythm of openings.*
- e. *Framing should be added to openings to mimic windows. The framing should have vertical members to deemphasize the horizontal lines of the structure.*
- f. *Landscaping should be used to screen and enhance the structure. Landscaping and vines planted on structure façades can help reduce the visual impact of the structure while berms and trees planted at the perimeter of the garage can screen lower levels.*
- g. *Where retail is not provided on the ground floor, the structure should be surrounded by landscaping so that the structure does not directly abut paved areas. A minimum of a 5' landscaping strip should be provided between paved areas and the structure.*
- h. *Parking structure lighting should be appropriately shielded so as not to spill into adjacent residential areas.*
- i. *Refer to Chapter 17.70 for additional standards on parking structures.*



Parking structure facade is broken up with variation in massing.



Horizontal openings broken up with vertical columns and framing.



Varied materials and colors.

AREA WIDE DESIGN GUIDELINES



Projecting sign with wrought iron mount.



"A" frame restaurant signage.

2.8.17 BUILDING SIGNAGE

Building signage is necessary for any commercial district to be successful. Chapter 17.80 of the Zoning Ordinance outlines the different types of building wall, freestanding and temporary signs a business may erect. However, downtown areas have unique signage needs compared to other retail and business areas. The items below are specific signage standards for Downtown El Monte. However, Chapter 17.80 of the Zoning Ordinance shall apply to items not specified below.

a. *Building Signs, Pedestrian-oriented. All businesses with a separate street entrance in the Gateway Specific Plan and in the Main Street and Zocalo Subareas of the Main Street Specific Plan shall have a minimum of one (1) pedestrian-oriented building sign with the business name. This may include one (1) or more of the following:*

- *Marquee or projecting sign with vertical clearance between 8' and 9' and a maximum height of 12' or the height of the parapet wall, whichever is lower.*
- *Canopy or awning sign.*
- *Hanging sign.*
- *Permanent window sign.*

b. *Painted Wall Signs:*

- *Shall only be permitted on the rear building façade.*
- *Shall not cover more than 20% of the surface on which the sign is painted.*

c. *Outdoor A-frame Signs. Shall be permitted for a food and beverage establishment subject to the following:*

- *Number of signs. A maximum of one (1) sign shall be permitted at the main entrance facing the public street. A second sign may be permitted if the business has a secondary public entrance facing an alley or parking area behind the building.*

- *Maximum size. Shall not exceed 6 sf in surface area per side for a single- or double-sided A-frame sign.*
 - *Maximum height. Shall not be greater than 4'.*
 - *Location. Signs shall not be located more than 3' from the business frontage.*
- d. *Freestanding Signs:*
- *Pylon signs shall be prohibited.*
 - *Monument signs shall be prohibited along Main Street between Santa Anita Avenue and Ramona Boulevard and along Lexington Avenue between Main Street and Ramona Boulevard.*
 - *Monument signs may be permitted along all other streets subject to the provisions outlined in the Zoning Ordinance.*
- e. *Window Signs. Permanent window signs shall not occupy more than 25% of any individual window. In addition, permanent window signs shall not occupy more than 15% of the combined area of all windows along each facade.*

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DOWNTOWN
MAIN STREET

Chapter 3
MOBILITY AND BEAUTIFICATION



Chapter 3 - Mobility and Beautification

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3. MOBILITY AND BEAUTIFICATION

3.1 INTRODUCTION

With the recent expansion of the El Monte Bus Station in close proximity to the existing El Monte Metrolink Station, the Specific Plan area has become a significant transit hub for the San Gabriel Valley and the larger Los Angeles region. These facilities provide residents and transit riders with daily access to an extensive network of regional, intra-state, and inter-state destinations. Together, both transit facilities provide the foundation for creating a more pedestrian-friendly, transit-oriented development center within Downtown El Monte.

One of the major challenges for implementing the multimodal vision for the Specific Plan area will be ensuring the integration of a complete streets network unique to Downtown El Monte. Complete streets are intended to enable safe access for all users including pedestrians, bicyclists, automobiles, and public transit riders. Individual streets within the Specific Plan area have been designated for specific groups of users and are discussed within this Section. These street types include:

- *Complete Streets (Pedestrians, bicyclists, vehicles, and public transit)*
- *Pedestrian, Bicycle, and Vehicle Streets*
- *Pedestrian Only Streets*

Through the enhancement of both the pedestrian, bicycle, vehicular, and public transit networks, this Specific Plan intends to increase the safety and accessibility of multiple modes of transit for all residents and visitors of Downtown while also making it an enjoyable place to live and work.

In addition to the proposed mobility enhancements, a number of public right-of-way and public space enhancements are proposed. Improvements to the public right-of-way are intended to foster a uniform character complementary to El Monte's existing context while also supporting potential new development. Public space enhancements include a number of proposed plazas that will provide existing and future residents with enjoyable spaces for community events, informal recreation, and entertainment.

This Chapter discusses the existing and proposed mobility and beautification improvements within the Specific Plan area in support the Guiding Principles (**Section 1.6.1**). A number of improvements related to transit, pedestrian access, streetscape, bicycle, and roadways for main thoroughfares, side streets, and alleyways are proposed and described in greater detail.



Metrolink provides regional access from Downtown El Monte.



Complete streets provide safe access for all users.

3.2 TRANSIT NETWORK

Downtown El Monte contains a number of existing public transit facilities served by various bus, commuter shuttle, commuter rail, and heavy rail providers on a daily basis that provide residents and visitors access to an extensive and robust transit network.

3.2.1 TRANSIT FACILITIES

El Monte Bus Station

The newly expanded El Monte Bus Station is located immediately west of the Specific Plan area and provides Downtown with multiple regional bus transit routes and services. The El Monte Bus Station currently serves the Silver Streak/J-Line with express bus service to Downtown Los Angeles, seven Metro bus routes, nine Foothill Transit bus routes, Norwalk Transit, Greyhound Bus lines and multiple shuttle services. The El Monte Bus Station has a large surface parking lot and a multi-level structure that offers a 1,760 space park-and-ride lot for commuters. The station also contains a Metro Bike Hub and provides a large number of public bicycle racks. In order to ensure impacts on existing public transportation services at the El Monte Bus Station are minimized, the following considerations should be adhered to during the implementation of the Specific Plan:

- Access to the El Monte Bus Station must remain as operated today from the intersection of Santa Anita Avenue and Ramona Avenue.
- While not within the Specific Plan area, the following should be considered in regards to the El Monte Bus Station:
 - Access to and the building footprint of Metro Division 9 must remain unchanged to accommodate the same number of buses as it does today;
 - Parking should be expanded and not be reduced; and
 - If there are potential route changes, temporary or permanent, as well as stops and layover relocations, the City must consult with Metro Stops and Zones to determine proposed impacts.

El Monte Metrolink Station

The El Monte Metrolink Station is located on the north end of the Specific Plan area adjacent to the City’s Trolley Station. The Metrolink Station provides 235 parking spaces for use by passengers and commuters.

3.2.2 EXISTING TRANSIT PROVIDERS

The Specific Plan area is serviced by several public transit operators that provide service to the greater Los Angeles region. These include:

Metropolitan Transportation Authority (METRO)

Metro is the Los Angeles County regional transit provider that offers a network of bus and rail lines within the County. Metro operates seven bus routes that have bus stops within the Specific Plan area, all of which serve the El Monte Bus Station.

Metrolink

Metrolink operates commuter heavy rail service throughout the Southern California area. Within the Specific Plan area, Metrolink operates the San Bernardino Line which stops at the El Monte Metrolink Station. This stop averages approximately 483 weekday passengers as of September 2013 (Metrolink).

Foothill Transit

Foothill Transit is the primary provider of local bus service in the San Gabriel Valley area of Los Angeles County. Express services to Downtown Los Angeles are also available to commuters, which provide connections to Orange and San Bernardino Counties, and other Metrolink commuter rail services. Similar to Metro, Foothill Transit operates nine (9) bus routes throughout the Specific Plan area that all stop at the El Monte Bus Station.

El Monte Transit/Commuter Shuttles

El Monte Transit is a local bus service that provides five routes throughout the City. In addition, the City provides a Commuter Shuttle loop route with stops at the El Monte Trolley Station, the El Monte Metrolink Station, Downtown El Monte and the El Monte Bus Station.

PROPOSED TRANSIT NETWORK IMPROVEMENTS

The wide variety of transit providers operating within the Specific Plan area are likely to continue at their current level of service and therefore minimal improvements to the existing transit network are proposed (see *Figure 3-1*). With the location of both the El Monte Metrolink and El Monte Bus Stations on opposite sides of the Specific Plan area, ensuring adequate connectivity between these stations via local providers will enhance local connection reliability for transit riders. Existing routes of the El Monte Trolley should be revised to bring greater access and visibility to Main Street. In addition, the supporting infrastructure to make existing transit stops and Downtown locations more accessible and identifiable to residents and visitors should be incorporated as recommended below and in **Section 3.6**.

- Future opportunities to relocate the El Monte Transit stop at Santa Anita Avenue on Ramona Boulevard to the El Monte Bus Station should be explored with Metro to improve transit rider convenience and safety.*
- The City’s Trolley Station current location adjacent to the El Monte Metrolink Station is temporary. A permanent location should be identified and it should be conveniently located in the Downtown Area.*
- Metro should coordinate with the City on a parking needs assessment study to determine whether a parking structure is needed at the Metrolink Station to ensure current SCRRA standards are met.*
- The Ramona Transit Plaza should be enhanced with streetscape treatments, landscaping, and wayfinding signage.*

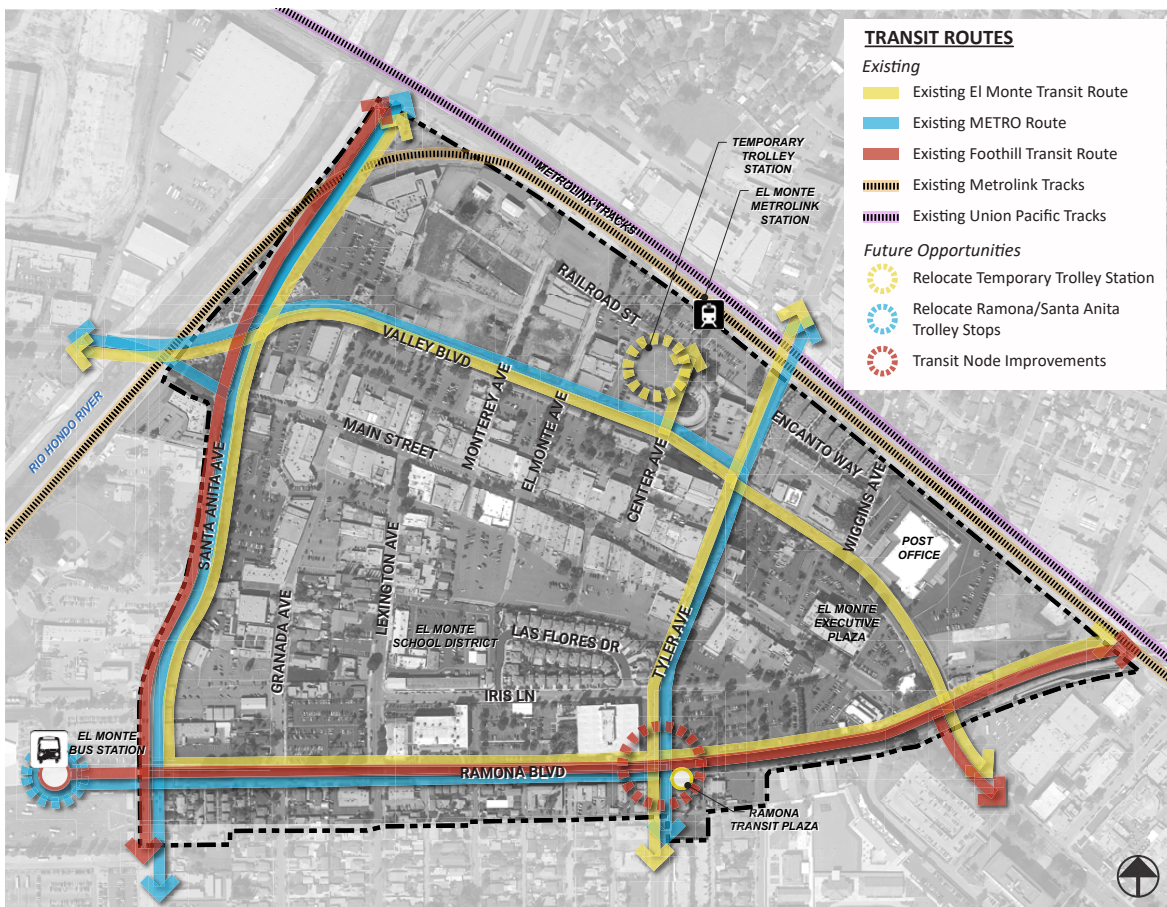


Figure 3-1 Transit Routes Map



Recent streetscape enhancements along Main Street.



Wide sidewalks enhance pedestrian accessibility.



Enhanced pedestrian and bicycle safety crossings.

3.3 PEDESTRIAN NETWORK

Downtown El Monte’s existing pedestrian network provides residents and visitors with a viable framework for creating a safe and livable environment within the City’s core. The network of existing sidewalks, paseos, and plazas enables pedestrian connectivity throughout the Downtown while also providing linkages to an extensive public transportation network. As new development occurs within the Downtown area, sidewalks, paseos, and plazas will need to be enhanced and expanded to ensure they can accommodate increased pedestrian activity while also remaining safe and efficient to use.

The sidewalk, paseo, and plaza improvements outlined in this Section are intended to improve the character and environment of the existing pedestrian network within the Specific Plan area. The guidelines provided should inform the development of the overall vision for Downtown as well as individual projects within the Specific Plan area.

3.3.1 SIDEWALKS

Sidewalks are provided throughout the Specific Plan area but some locations are currently inadequate to accommodate existing pedestrian circulation within the Downtown area. To ensure safe and efficient connections between various public transportation facilities and to enhance pedestrian safety, **new development and redevelopment projects on Valley Boulevard, Santa Anita, and Ramona Boulevard should increase sidewalks to a minimum of eight (8) feet in width.**

3.3.2 PEDESTRIAN CROSSINGS

The existing pedestrian crossings within the Specific Plan area include several unmarked and unsignalized crossings at key Downtown intersections. These existing conditions create an environment that discourages walking and places a greater emphasis on vehicular traffic. In order to enhance the overall safety and walkability within the Specific Plan area while also shifting to more balanced, multimodal streets, a number of pedestrian safety and pedestrian/bicycle priority crossings have been proposed. Pedestrian safety crossings have been designated at specific intersections based on existing roadway characteristics and lack of appropriate

pedestrian supporting safety provisions. Pedestrian/ bicycle crossings are signed and signaled multimodal intersections that provide longer crossing times for pedestrians and bicyclists and have been designated to provide more direct paths of travel between high traffic destinations within the Specific Plan area and those located elsewhere in the City. The following pedestrian crossing improvements (Figure 3-2) are recommended for the Specific Plan area:

- b. Integrate pedestrian safety crossing enhancements at Tyler Avenue and the Metrolink railroad line that include Metrolink’s newest grade crossing safety standards including, but not limited to pedestrian gates, flashers, and railings and channelization.
- c. Incorporate bicycle/pedestrian priority crossings at the intersections of Santa Anita Avenue and Main Street, Santa Anita Avenue and Ramona Boulevard, Ramona Boulevard and Tyler Avenue, Tyler Avenue and Main Street, Valley Boulevard and Center Avenue, and the five-way intersection at Valley Boulevard, Main Street, and Ramona Boulevard.
- d. Bicycle/pedestrian priority crossings should include stamped and/or highly visible crosswalks and a vehicular stop line setback from the crossing.

- a. Incorporate pedestrian safety crossing enhancements at the intersections of Valley Boulevard and El Monte Avenue and Lexington Avenue and Ramona Boulevard as well as the other locations along Santa Anita Avenue and Valley Boulevard (see Figure 3-2) including stamped and/or highly visible crosswalks, flashing pedestrian crossing signage, pedestrian lights, and a vehicular stop line setback from the crossing.

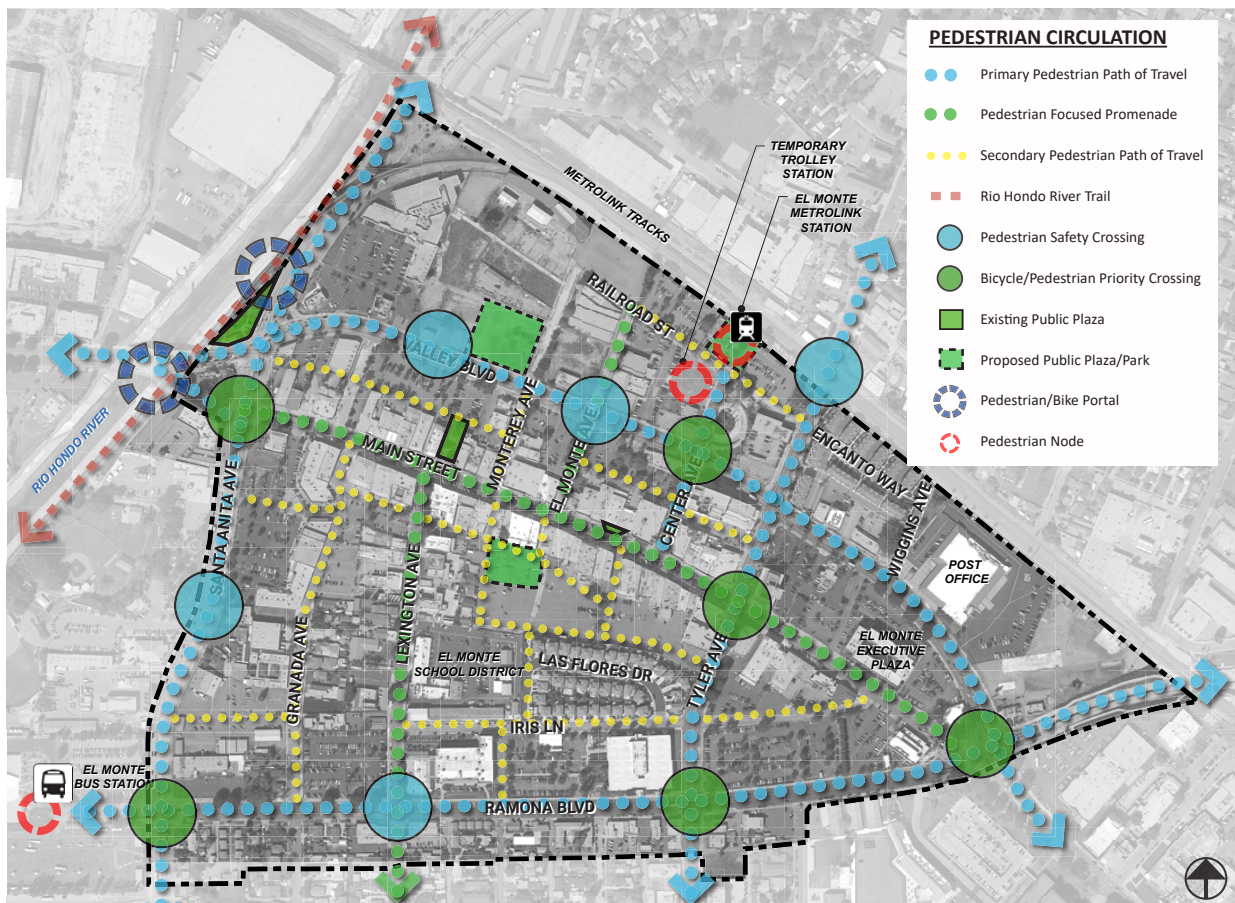


Figure 3-2 Pedestrian Network Map

3.3.3 PLAZAS & PARKS / OPEN SPACE

In order to support the health and wellness of the larger community, additional public spaces and convenient urban recreation options will need to be provided within the Downtown area. Residents voiced this desire at community workshops and indicated a strong preference for incorporating places to gather, celebrate, and have community events within the Specific Plan area.

A number of strategic locations have been identified within the Downtown area that could be developed as plazas and parks/open space areas in conjunction with the evolution of the Specific Plan area. These public plazas and parks/open space areas will enhance the Downtown environment by fostering a sense of place and identity while at the same time providing enjoyable spaces for community events, informal recreation, and street side entertainment. All plazas and parks/open space areas incorporated within the Specific Plan area should adhere to the following guidelines:

- a. *New plazas should be located within the Downtown area as shown in Figure 3-3.*
- b. *The existing plaza located on Main Street, west of Monterey Avenue, should be improved through the incorporation of additional amenities (Figure 3-3).*
- c. *Public plazas should be purposefully designed to provide opportunities for human activity for all age groups and an interactive environment that provides spaces for events, entertainment, and gatherings while also fostering a sense of community within the Downtown area.*
- d. *Public plazas should be designed with flexibility for physical use and be located to accommodate a range of desired activities, such as outdoor seating, concerts, and festivals.*
- e. *Plazas should have an articulated edge (buildings, benches, landscaping) to define the plaza and create a comfortable space.*
- f. *Pedestrian amenities should be included such as seating, lighting, planters, fountains, drinking fountains, distinctive paving, public art, landscaping, and bicycle racks.*
- g. *Lighting within public plazas should be a maximum height of 20 feet.*
- h. *Larger projects should incorporate park/open space areas for public enjoyment and use.*



Figure 3-3 Existing and proposed public plazas



Plazas provide spaces for community gatherings and entertainment.



Fountains within plazas help define the space.



Existing pedestrian paseo along Main Street.



Shops opening onto a pedestrian paseo.



Pedestrian-friendly paseos allow for outdoor dining opportunities.

3.3.4 PEDESTRIAN PASEOS

Downtown El Monte has a number of existing pedestrian paseos located to the side of and behind businesses along Main Street. A majority of these paseos lack pedestrian amenities and do not facilitate connectivity to other parts of the Specific Plan area. In order to enhance the connectivity and walkability of the Specific Plan area, a pedestrian focused promenade has been proposed along El Monte Avenue connecting the Metrolink Station to the Downtown core (*Figure 3-4*). Both new and existing pedestrian paseos will be enhanced through the incorporation of paving accents, landscaping, signage, and wayfinding features to guide pedestrians through paseos from one location to the next. Existing and proposed pedestrian paseos and pedestrian focused promenades should adhere to the following guidelines:

- a. *El Monte Avenue should be transformed into a pedestrian focused promenade linking the Metrolink Station to the Downtown core. The existing pedestrian paseo near El Monte Avenue, south of Main Street, should be enhanced with pedestrian amenities as described in "Guideline d" below.*
- b. *Convert Palm Court into an improved pedestrian paseo and eliminate vehicular access.*
- c. *New pedestrian paseos should provide linkages between public parking and the street environment, residential projects and adjacent streets, and residential projects and the Downtown core.*
- d. *Pedestrian amenities such as seating, decorative lighting, wayfinding signage, planters, fountains, drinking fountains, distinctive paving, decorative tiles, public art, landscaping, potted plants and bicycle racks should be incorporated to enhance the paseo environments. Focal points within pedestrian paseos should be incorporated, such as water features or sculptures, where appropriate.*
- e. *Pedestrian paseos should allow for vendors and outdoor sales that are in heavily trafficked and highly visible locations.*
- f. *Public art should be integrated along the pedestrian paseo network and also allow for temporary art installations featuring local artists.*



Focal points along a pedestrian paseo.



Formal gathering spaces within pedestrian paseos.

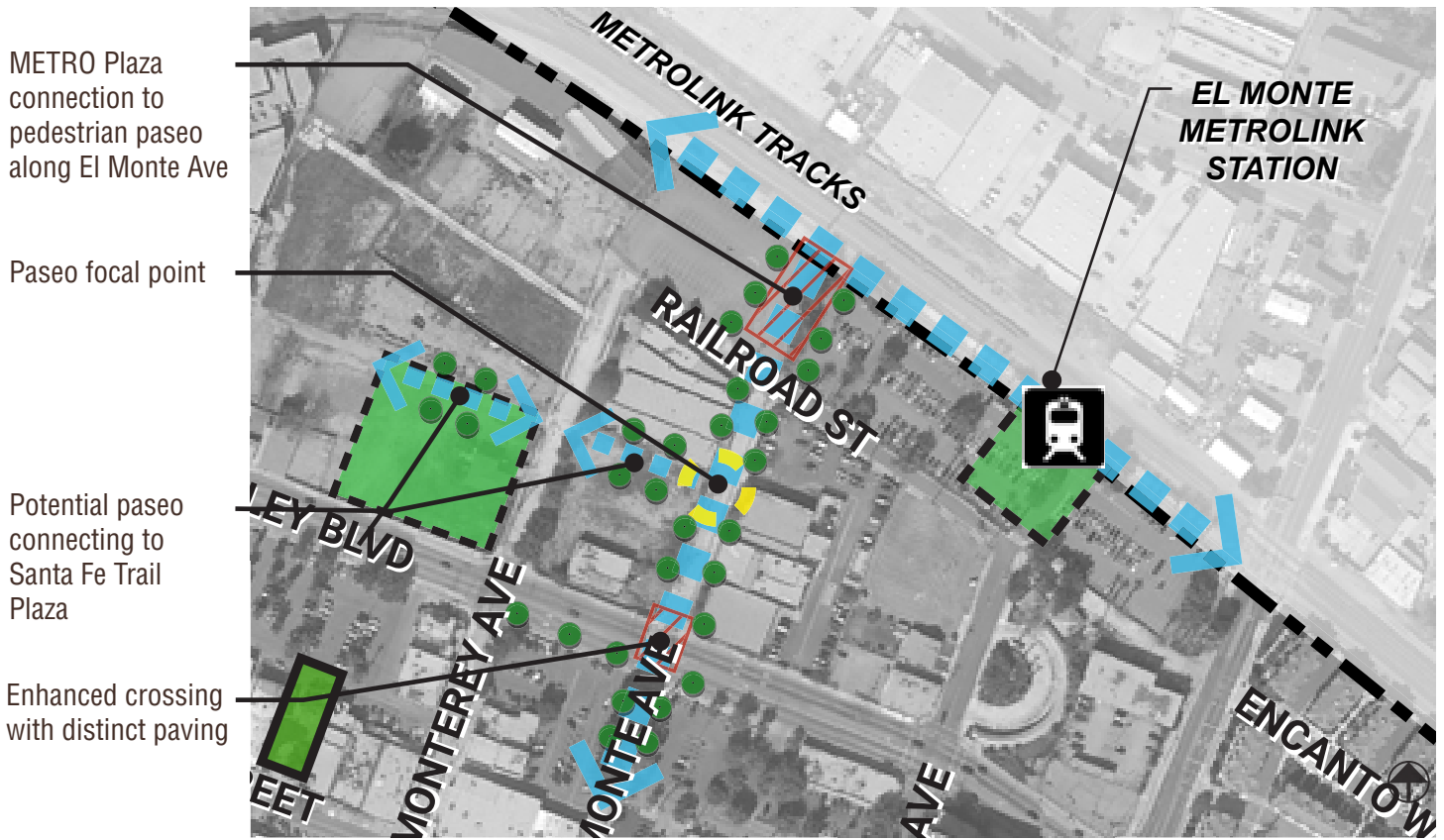


Figure 3-4 Conceptual Pedestrian Focused Promenade along El Monte Avenue.



Existing alley paseo condition abutting Main Street.



Alley paseos accommodate both pedestrian and vehicular access while providing additional pedestrian amenities.

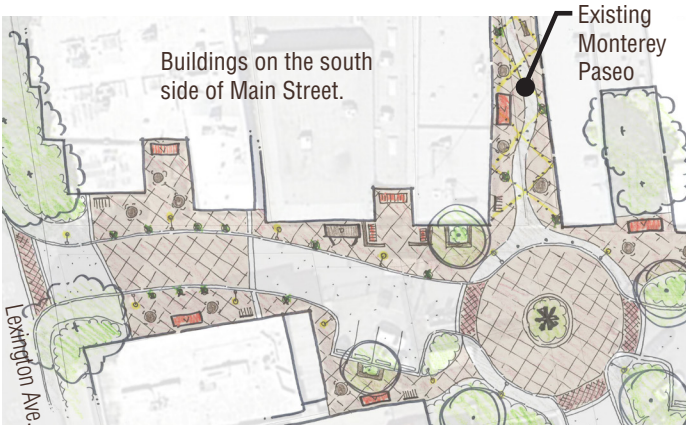


Figure 3-5 Conceptual enhancements along existing alley paseo in Downtown.

3.3.5 ALLEY PASEOS

With the addition of new residents and businesses within Downtown, the existing alleyway network will need to evolve into a highly-functioning, shared resource accommodating multiple users. This will require a number of design considerations and improvements as outlined below. All alley paseos within the Specific Plan area should adhere to the following guidelines:

- a. *As alley paseos are redesigned to better accommodate both pedestrians and vehicles, the following considerations should be incorporated:*
 - *Integrate a pedestrian walkway with attractive and unique paving;*
 - *Utilize stamped asphalt or unit pavers instead of standard asphalt for vehicle access;*
 - *Locate landscape to add to the comfort of pedestrian users while minimizing interference with service access for local businesses;*
 - *Provide additional lateral movement for vehicles to discourage cut through traffic; and*
 - *Limit the speed of vehicles to a maximum of 20 mph through the redesign of physical space.*
- b. *Provide pedestrian amenities such as seating, decorative pedestrian-scaled lighting, wayfinding signage, landscaping, and bicycle racks within alley paseos.*
- c. *Improve the environment of the alley paseos by concealing and consolidating dumpsters and providing screening for utilities.*
- d. *Businesses should provide business signage on the facades fronting onto alley paseos consistent with Chapter 17.12 of the Municipal Code.*
- e. *Businesses fronting onto a street and abutting an alley paseo should incorporate a side or rear entrance as an alternative to the main entry.*
- f. *Improve alley and paseo connection on the southeast corner of the Main Street/Lexington Avenue intersection (see Figure 3.5).*

3.4 BICYCLE NETWORK

3.4.1 EXISTING CONDITIONS

Another goal of the Specific Plan is to transform the Downtown into an environment where public transportation, retail establishments, and entertainment venues are easily accessible through walking as well as bicycling. Bicycle infrastructure is currently limited within the Specific Plan area. Those who choose to ride their bicycles are forced to do so under unsafe conditions often utilizing sidewalks to navigate more efficiently. Ensuring the integration of a range of Class I, II, and III bicycle lanes within or adjacent to the Specific Plan area will help create the safe, interconnected, and multimodal circulation envisioned for the Downtown. Additionally, by providing pedestrian/bicycle portals, serving as community identifiers, local bicycle infrastructure can be connected to existing regional bicycle infrastructure to provide alternative transportation mode options.

Emerald Necklace

Since the early 2000’s, the Amigos de los Rios, non-profit groups, and multiple cities have worked together to create and implement their vision for a regional bicycle network connecting parks and open spaces throughout the San Gabriel Valley. Known as the Emerald Necklace, the Emerald Necklace Expanded Vision Plan (see *Figure 3.6*) envisions connecting the Los Angeles region, spanning from the Angeles National Forest in the north with the Pacific Ocean and Catalina Island in the west, through an integrated network of bicycle and pedestrian pathways. A portion of the Emerald Necklace runs directly adjacent to the Specific Plan area along the Rio Hondo River, which currently contains the Class I Rio Hondo River Trail. To ensure proper access to the Emerald Necklace via the Rio Hondo River Trail is provided, the Specific Plan proposes a number of bicycle related circulation, trail access portals, and signage improvements that are intended to further the goals and priorities outlined within the Emerald Necklace Expanded Vision Plan while also making the Downtown area more bicycle friendly.

Source: Amigos de los Rios



Figure 3-6 2005 Emerald Necklace Plan

3.4.2 PROPOSED BICYCLE NETWORK IMPROVEMENTS

Proposed bicycle network improvements are intended to foster improved access and connectivity within the Specific Plan area to the rest of El Monte while also improving access to the Emerald Necklace via the existing Rio Hondo River Trail.

Bike Facilities

The 2014 San Gabriel Valley Regional Bicycle Master Plan has recommended the following improvements for the Specific Plan area which are currently identified and expanded upon in *Figure 3-7*. Proposed improvements will support increased bicycle and transit use by providing

safer access between the El Monte Bus Station, the Metrolink Station, the Rio Hondo River Trail, and other destinations within and adjacent to Downtown El Monte. Bicycle facility improvements within the Specific Plan area should include the following:

- a. Class II (Striped) on Valley Boulevard, Tyler Avenue north of Ramona Boulevard, and portions of Ramona Boulevard
- b. Class II (Buffered) on Santa Anita Avenue north of Valley Boulevard.
- c. Class III with sharrow (shared lane) stencil markings on roads designated in Figure 3-7.
- d. Class III Bike Boulevards on El Monte Avenue, Main Street, and Lexington Avenue. Bike Boulevards should be optimized for bicycle travel

using traffic calming and reduction, signage and pavement markings, and intersection crossing treatments.

- e. Pedestrian/Bicycle crossing improvements as identified in **Section 3.3.2**.
- f. Pedestrian/Bicycle Portal enhancements including additional signage, lighting, and pavement markings.
- g. Metro Bike Hub program, currently located at the El Monte Bus Station, should be replicated to the El Monte Metrolink Station.

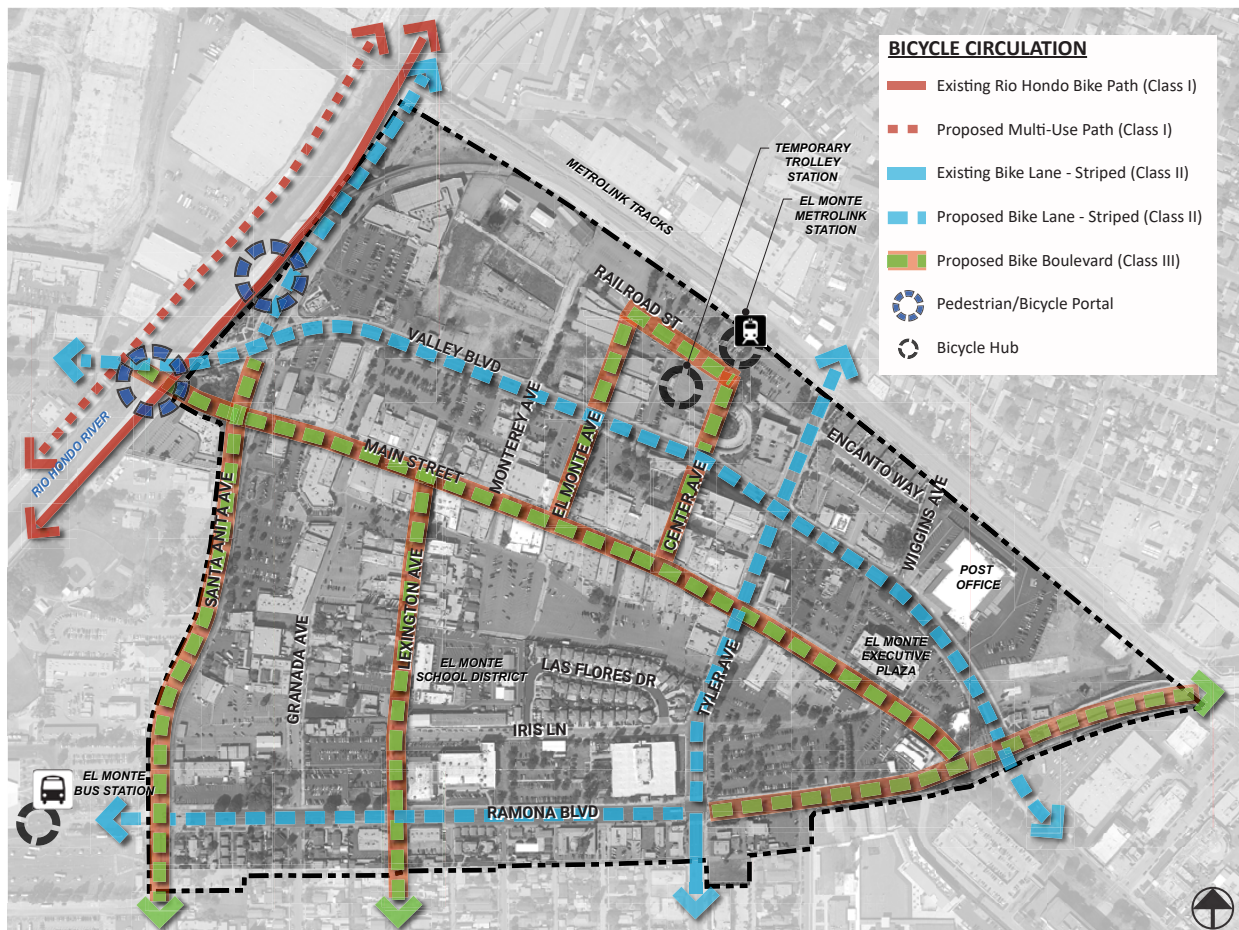


Figure 3-7 Bicycle Circulation Map

3.5 ROADWAY NETWORK

With the exception of Santa Anita Avenue and Main Street, the existing roadway network configuration within the Specific Plan area will remain largely unchanged. Santa Anita Avenue and Main Street are the focus of this Section and are discussed in greater detail in order to ensure adequate enhancements are implemented that are specific to each roadway. In addition to the streetscape improvements discussed in **Section 3.6**, street specific improvements for prominent roadways within the Specific Plan area are also outlined below.

3.5.1 SANTA ANITA AVENUE IMPROVEMENTS

Located on the western edge of Downtown, this roadway intersects Main Street just south of Valley Boulevard and has been the location of more recent developments including the El Monte Gateway project. The El Monte Station is also located along Santa Anita Avenue, which defines a large portion of the western edge of the Specific Plan area. The following recommendations have been made to specifically enhance Santa Anita Avenue within the Specific Plan area:

- a. *Santa Anita Avenue should be modified to become a Complete Street within the Specific Plan area, providing accommodations for pedestrians, bicycles, automobiles, and public transit.*
- b. *Left turn movements currently exist on southbound Santa Anita Avenue into eastbound Main Street and left turn movements on westbound Main Street onto southbound Santa Anita Avenue. Any proposed changes to this configuration should involve the City and DEMBA.*
- c. *The following list of street improvements should be implemented along Santa Anita Avenue:*
 - *Share the Road bicycle improvements (stencil)*
 - *Landscaped median*
 - *Additional street trees*
 - *Widened sidewalks with parkway landscaping*
 - *Enhanced crosswalks and intersections*
 - *Improved signalization*

3.5.2 MAIN STREET IMPROVEMENTS

Main Street has the character and presence to become the heart of the El Monte community. Currently Main Street is active from 9 am to 5 pm but nearly vacant during evening hours. One of the primary goals of this Specific Plan is to enhance the existing character and extend vibrant daytime activity of Main Street into the evening hours, by reprogramming uses, extending business hours, and promoting Downtown as a local destination. The following recommendations have been made to specifically enhance Main Street within the Specific Plan area:

- a. *Valley Mall should be renamed to Main Street to better reflect the character and environment of the Downtown core to both residents and visitors of El Monte.*
- b. *While a number of new trees and landscaping were recently installed on Main Street, strategic reworking of the hardscape and minimal landscape reconfiguration should occur as outlined in detail within Figures 3-8 through 3-10.*
- c. *Streetscape improvements should be incorporated along Main Street as outlined within **Section 3.6**.*
- d. *All future tree and landscaping should be selected from the landscape palette provided in **Section 3.6.13** while playing off the existing landscape.*
- e. *While no roadway reconfigurations are proposed for Main Street, the section of Main Street from Monterrey Avenue to Center Avenue should be repaved with accent paving and include removable bollards to allow for temporary events to occur. This treatment could be expanded further over time to allow additional flexibility in size of event or street closure.*

3.5.3 OTHER IMPROVEMENTS

The project area is also within close proximity to the I-10 and I-605 corridors, the El Monte Bus Station, and El Monte Metrolink Station. Therefore, the following recommendations have been made for larger projects to improve access for vehicles, transit users, and pedestrians:

- a. *Traffic Mitigations. Require cost of transportation mitigations and improvements needed for new*

- development to be borne by applicants. For mitigation project required for regional significant projects, developers shall pay a fee to help fund a project-specific report.
- b. The City will work with neighboring cities to address cumulative significant traffic impacts on I-10, I-605, and on/off-ramps as a result of buildout of the Specific Plan.
 - c. The City will work with Caltrans to identify potential cumulative traffic impacts and mitigation measures.
 - d. The City will form a fair share fee program working with neighboring cities to improve the State facilities.
 - e. The City's existing traffic impact fees will include any State facility improvement as part of the cumulative traffic impact. Procuring funds toward freeway segments, freeway interchange, freeway on/off ramps as well as for bus and rail transit facilities shall be part of the goals of the City.
 - f. The City may accept fair share funding contributions towards future improvement of the State facilities so long as Caltrans can show that such improvements are reasonably expected to be implemented in a reasonable time frame. The City shall contact Caltrans to explore and develop these reasonable measures and plan.

Figure 3-8 through Figure 3-10 provide examples of specific improvements for streets identified within **Section 3.5**.



Figure 3-8 Proposed Main Street Improvements (Santa Anita Avenue to Monterey Avenue).

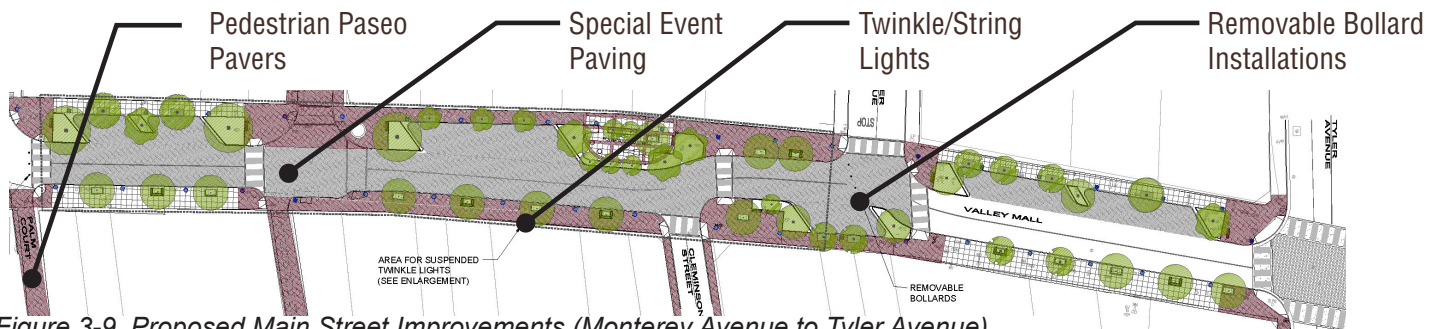


Figure 3-9 Proposed Main Street Improvements (Monterey Avenue to Tyler Avenue).

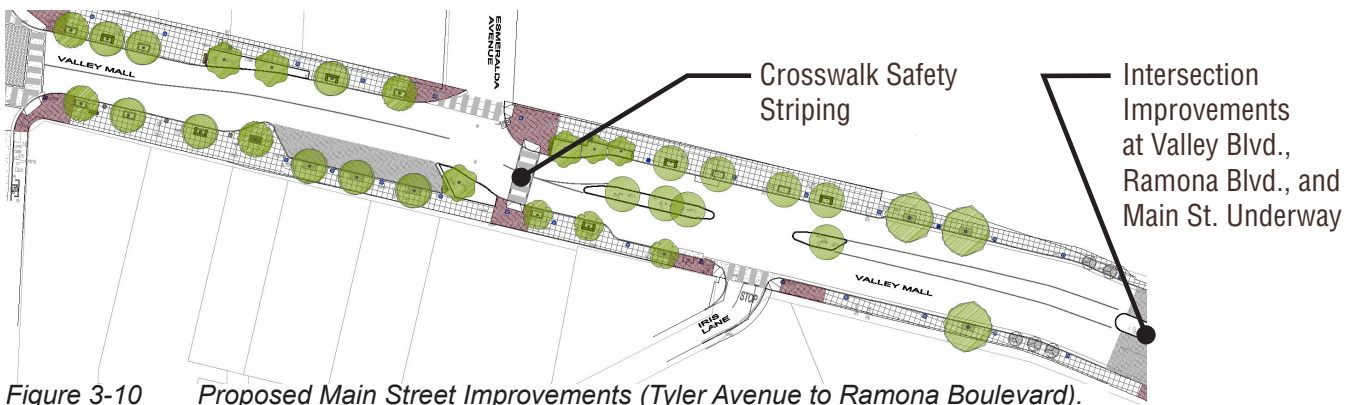


Figure 3-10 Proposed Main Street Improvements (Tyler Avenue to Ramona Boulevard).

3.6 STREETScape BEAUTIFICATION

Streetscape improvements are a necessary component of an active street life and can provide the medium for enabling informal interactions within the public realm. Streetscape furnishings also aid in the establishment of an identity for a Downtown when cultural or historic motifs are included within design expressions. Improvements such as benches, bus shelters, trash receptacles, plant containers, bicycle racks, bollards, and kiosks all present opportunities to express the unique identity of El Monte.

This Section proposes to build upon the existing streetscape improvements within the Specific Plan area in order to enhance the comfort, safety, and active transportation needs of current and future Downtown residents and visitors. While the proposed streetscape improvements will vary slightly for individual areas, they are intended to collectively work together to establish a recognizable character that is specific to Downtown El Monte.

The Specific Plan area has been divided into two separate areas to allow for individual streetscape beautification in conjunction with roadway network improvements (see **Section 3.5**). Area One has been designated to provide a specific focus on Lexington Avenue, envisioned as an extension of Main Street, El Monte Avenue, envisioned as a pedestrian promenade, and Main Street itself; establishing a strong, pedestrian-oriented backbone through the Downtown. Area Two includes all other areas not specifically identified in Area One and is intended to establish complementary streetscape improvements to those in Area One. The following recommendations identify Streetscape Beautification Improvements for both Area One and Area Two:

Streetscape Beautification Improvement Areas

Area One: Main Street, Lexington Avenue, El Monte Avenue, Plazas, and Pedestrian Paseos (see *Figure 3-11*)

Area Two: All other Specific Plan areas

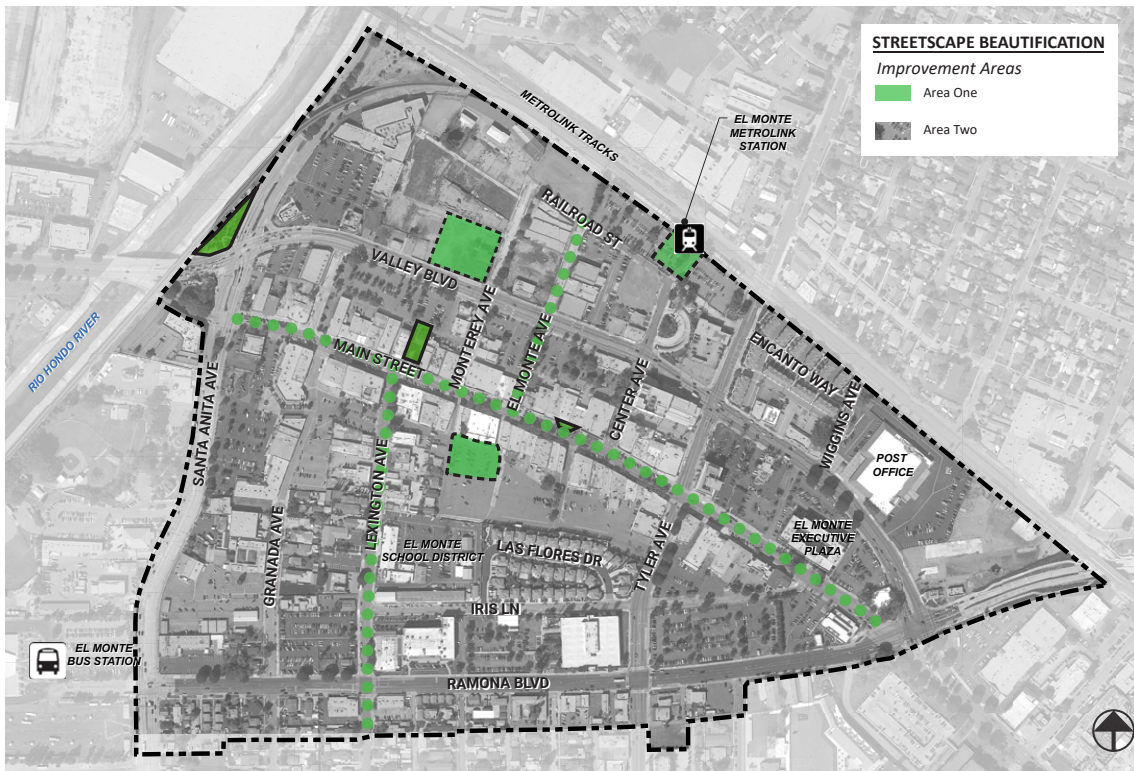


Figure 3-11 Streetscape Beautification Areas

3.6.1 PRIMARY ENTRYWAYS

Primary entryways are located at intersections where proposed streetscape improvements will inform drivers and pedestrians of their arrival into Downtown. Within the Specific Plan area, primary entryways have been identified along Main Street at the intersections of Santa Anita Avenue, Tyler Avenue, and Ramona Avenue as well as along Valley Boulevard at the intersections of Tyler Avenue and Ramona Boulevard (see *Figure 3-12*). Intersections identified as primary entryways should incorporate the following recommendations:

- a. A combination of the following accent features should be incorporated into primary entryways: ornamental landscaping, landscaped medians, architectural features on adjacent buildings (such as tower elements), decorative walls, signage, or enhanced paving.
- b. The use of colored, textured, and permeable paving treatment at intersections and entry drives is encouraged.

3.6.2 SECONDARY ENTRYWAYS

Intersections that provide neighborhood orientation and alternative access points within Downtown are considered secondary entryways. Secondary entryways within the Specific Plan area have been identified along Santa Anita Avenue connecting Downtown to the new Gateway Specific Plan to the west, on Main Street at the intersections of Granada Avenue, Lexington Avenue, Monterey Avenue, El Monte Avenue, and Center Avenue as well as along Valley Boulevard at the intersections of El Monte Avenue and Center Avenue, at the intersection of Ramona Boulevard and Lexington Avenue, and along Tyler Avenue at Ramona Boulevard and the railroad tracks (see *Figure 3-12*). Intersections identified as secondary entryways should adhere to the following:

- a. A combination of the following accent features should be incorporated into secondary entryways: enhanced paving, pedestrian lighting, and wayfinding signage.



Figure 3-12 Entryway Intersection Locations



Existing Bench within Downtown.



Example of Re-purposed Pre-Cast Concrete Benches.



Victor Stanley CBF-318 Bench.
(Source Victor Stanley)

3.6.3 BENCHES

Two bench styles are recommended for installation within different areas of the Specific Plan.

Area One:

The existing pre-cast concrete benches located within Downtown will remain in place and be re-purposed as part of a public art project.

- a. *As funding becomes available, existing pre-cast concrete benches within the Downtown area should be clad with ceramic or other equally durable materials that portray and celebrate El Monte's culture, history, or the unique stories of local residents as a public art project. An example of a similar art project undertaking is shown to the left.*
- b. *New pre-cast concrete benches should be located along Lexington Avenue, El Monte Avenue, in plazas, and along pedestrian paseos as they are redeveloped.*

Area Two:

For all other areas the Victor Stanley CBF-318 (or similar) bench should be installed as shown below.

- a. *Benches should be placed a minimum of 300 feet apart to provide convenient resting places along streets within the Specific Plan area.*
- b. *Benches should be located with backs against buildings or with backs perpendicular to street circulation.*
- c. *Where space allows, benches should be clustered with trash receptacles and other similar street furnishings.*

3.6.4 TREE WELLS

While trees provide a number of benefits to urban environments, without proper design and placement, they can create access and safety issues for pedestrians.

- a. *All trees planted along sidewalks and in paseos and plazas within the Specific Plan area should utilize tree wells that are 4'x4' to expand the usable sidewalk area.*

- b. *Newly planted trees should be staked for support with the root ball covered with 4" of decomposed granite compacted so that it is flush with surrounding paving.*

3.6.5 BICYCLE RACKS

Bicycle racks strategically located within and adjacent to transit stops, commercial areas, civic areas, and parking lots encourage bicycle ridership and provide an attractive alternative to locking bicycles to trees and light poles.

- a. *All bicycle racks installed within the Specific Plan area should utilize the DERO Bike Hitch model (or similar) powder coated in Hunter Green or Flat Black, as shown to the right.*
- b. *Along Main Street, a set of three bicycle racks should be provided on each side of street for every block between Tyler Avenue and Santa Anita Avenue.*
- c. *Bicycle racks should be located within or adjacent to all existing and proposed plazas and pedestrian paseos and high ridership bus shelters/stops.*

3.6.6 BUS SHELTERS

Bus shelters and transit stops play an important part in informing users of stop locations and ensuring coverage from the elements. Ensuring a clean, uncluttered appearance makes shelters and stops more inviting and encourages greater use.

- a. *All bus shelters should be consistent with the existing style located within the Specific Plan area as shown to the right.*
- b. *All bus stops should provide trash receptacles, transit schedules, and sufficient lighting for safety.*

3.6.7 BOLLARDS

Bollards may be located where restriction to vehicular movement is desired such as intersections to prevent traffic from encroaching on areas of high pedestrian activity or where temporary events will occur.

- a. *All bollards utilized within the Specific Plan area*



Proposed Metal Tree Wells.



*DERO Bike Hitch.
(Source DERO)*



Existing City Bus Shelters.



Example of Removable Bollards.



Global Tap GT1000 Drinking Fountain.



Victor Stanley S-42 Trash Receptacle.
(Source Victor Stanley)

should have a similar style to the existing historic street light base and be black in color.

- b. Temporary bollards should be located along Main Street to allow for temporary events to occur such as concerts or farmers markets.*

3.6.8 DRINKING FOUNTAINS

Drinking fountains provide residents and visitors convenient access to water for refilling of reusable water bottles or a quick drink on a hot day.

- a. All drinking fountains within the Specific Plan area should utilize the GlobalTap GT1000 Bottle Filler model in blue (or similar) and should be placed at locations of community activity such as plazas and paseos.*

3.6.9 TRASH RECEPTACLES

Trash receptacles located throughout the Specific Plan area will ensure a clean and neat appearance for the Downtown. While concrete trash receptacles currently exist along Main Street, future installations will consist of the recommended product type below.

- a. All trash cans within the Specific Plan area should utilize the Victor Stanley S-42 Trash Receptacle model in black as shown to the left (or similar).*

3.6.10 POTTED PLANTS

Potted plants will be allowed to be placed and maintained by retail/commercial business within Area One.

- a. Potted plants may be placed on sidewalks with saucers to capture water as long as they are placed against the exterior walls of the business.*
- b. Potted plants should not block any required accessible pathway and at a minimum should provide 44" clearance.*

3.6.11 ACCENT PAVING

Pervious, interlocking pavers should be used at designated accent paving areas. Accent paving materials should be installed in all of the following locations:

- a. **Sidewalks** – At corner locations and along the section of Main Street from Santa Anita Avenue to Ramona Boulevard.
- b. **Crosswalks** – High visibility patterns shall be used on crosswalks as discussed in **Section 3.3** and **Section 3.5**.
- c. **Alley Paseos** – Along sections where pedestrians and vehicles are utilizing the shared network as discussed in **Section 3.3**.

3.6.12 LIGHTING

To reinforce El Monte's historic image, the traditional pedestrian lighting style existing along Main Street will be utilized in all areas of the Specific Plan.

Pedestrian Lights

Pedestrian lights are typically focused on pathways, creating smaller circles of light that illuminate sidewalk areas and provide for additional safety and security. Other types of pedestrian lighting include string lighting which provides additional definition and detail to outdoor dining, paseos, and plaza spaces.

- a. *The traditional pedestrian lights found along Main Street should be continued on all streets within Area One and Area Two.*
- b. *Traditional pedestrian lighting should also be located at mid-block crossings within Area One to help bring awareness to and illuminate the crossings.*
- c. *Existing spacing patterns for the traditional pedestrian lighting currently found along Main Street should be continued. All traditional pedestrian lighting should be placed approximately 40 feet apart.*
- d. *String or Twinkle lighting should be incorporated along Main Street, between Monterey Avenue and Center Avenue, and along pedestrian paseos. String and twinkly lights may also be incorporated to enhance outdoor dining, plazas, or other outdoor spaces within the Specific Plan area.*



Accent Paving.



Existing Pedestrian Lights along Main Street.



String and Twinkle Lighting.

3.6.13 LANDSCAPING

Tree, shrubs, perennials, and groundcover plantings for the Specific Plan area have been selected based on climate, drought tolerance, durability, flower color, foliage texture/color, and general maintenance requirements. Plants from the following list should be selected when designing public spaces and planting public right-of-way improvements. While both native and climate adapted non-native plants have been listed, an emphasis should be placed on integrating water conserving plant species identified as having a very low or low plant factor. Plants not listed below are not excluded from use within the Specific Plan area but rationale should be provided if other plants are selected.



Chinese Elm.

Tree List

- Peppermint Tree (*Agonis flexuosa*)
- Strawberry Tree (*Arbutus unedo*)
- Ginkgo (*Ginkgo biloba*)
- Lemon Bottlebrush (*Callistemon citrinus*)
- Western Redbud (*Cercis occidentalis*)*
- Royal Purple Smoke Tree (*Cotinus coggygria* 'Purpureus')
- GoldenRain Tree (*Koelreuteria paniculata*)
- Palo Verde (*Cercidium* 'Desert Museum')
- Pink Melaleuca (*Melaleuca nesophila*)
- Swan Hill Olive (*Olea europaea* 'Swan Hill')
- Chinese Elm (*Ulmus parvifolia*)
- London Plane Tree (*Platanus x acerifolia* 'Columbia')
- Sawleaf Zelkova (*Zelkova serrata* 'Village Green')
- Sweet Bay (*Laurus nobilis* 'Saratoga')

Shrub List

- Agave (*Agave attenuata*)
- Aloe (*Aloe species*)
- Manzanita (*Arctostaphylos species*)*
- Little John Bottlebrush (*Callistemon viminalis* 'Little John')
- Wild Lilac (*Ceanothus species*)*
- Rockrose (*Cistus species*)
- Australian Fuchsia (*Correa species*)
- Canyon Prince Wild Rye (*Leymus condensatus* 'Canyon Prince')*
- Blue Fescue (*Festuca* 'Siskiyou Blue')
- Red Yucca (*Hesperaloe parviflora*)
- Lavender (*Lavandula species*)
- Reevesii Tea Tree (*Leptospermum laevigatum* 'Reevesii')

- Oregon Grape (*Mahonia aquifolium*)*
- Montra Olive (*Olea europaea* 'Montra')
- Redberry Rhamnaceae (*Rhamnus crocea*)*
- Rosemary (*Rosmarinus species*)
- Sage (*Salvia species*)
- Germander (*Teucrium chamaedrys*)
- Adam's Needle Yucca (*Yucca filamentosa* 'Adam's Needle')
- Blue Hibiscus (*Alyogyne huegelii*)
- Hens and Chicks (*Echeveria species*)
- Kangaroo Paw (*Anigozanthus hybrids*)
- Toyon (*Heteromeles arbutifolia*)*
- Texas Ranger (*Leucophyllum frutescens*)
- Grevillea (*Grevillea species*)



Little John Bottlebrush.



Canyon Prince Wild Rye.

Groundcover

- Manzanita (*Arctostaphylos species*)*
- Wild Lilac (*Ceanothus species*)*
- Lantana (*Lantana species*)
- Creeping Mahonia (*Mahonia repens*)*
- Rosemary (*Rosmarinus species*)
- Stonecrop (*Sedum species*)
- Verbena (*Verbena peruviana*)

* **Indicates California native on existing list**



Mexican Sage.



Sidewalk Mosaic as Public Art



Public Art Installation

3.6.14 PUBLIC ART

The display of public art is an important part of expressing the history, personality, and character of a community. Public art can vary in scale, which can include small tile banding on a stair riser or larger pieces such as interpretive sculptures and/or functional art. Public art can also be used as a wayfinding feature to attract pedestrians to key locations such as a plaza or paseo or expressed in streetscape improvements such as paving, benches, or street lights within the public right-of-way.

Downtown El Monte provides multiple opportunities to enhance the Downtown environment through the incorporation of art in sidewalks, plazas, paseos, or other pedestrian spaces.

- a. *Public art should be located within the public plazas identified in **Section 3.3.3.** and at Primary Entryways identified in **Section 3.6.1.***
- b. *All existing and proposed pedestrian paseos should include spaces for public art, whether for permanent installations such as sidewalk mosaics or temporary installation pieces featuring local artists.*
- c. *As further discussed above in **Section 3.6.3,** the existing concrete benches should become part of a public art project and refurbished with ceramic or other equally durable materials.*

3.7 PUBLIC SIGNAGE AND WAYFINDING

An effective wayfinding and public signage system is an important component of ensuring residents and visitors can effectively navigate and locate their intended destination within Downtown El Monte. Public signage should contain a unified color scheme, logo, and materials palette. By implementing a public signage and wayfinding system, the City will create a recognizable theme and identity for Downtown that will facilitate informed access and circulation while also improving marketability and economic vitality.

The conceptual signage program detailed within this Section builds off the existing historic character of Downtown El Monte while also including ideas for new street signs. Examples shown need further exploration before being implemented and installed but are intend to illustrate signage types and potential design characteristics for the purposes of enhancing the Downtown environment. In addition, signage goals provided create a framework for implementation of a public signage and wayfinding program.

3.7.1 PUBLIC SIGNAGE AND WAYFINDING GOALS

The following guidelines are intended to provide a cohesive approach to public signage and wayfinding within Downtown El Monte.

- a. *Create wayfinding and entry signage that is clear, concise, and easy to read.*
- b. *Include directional signs that reflect design materials and components of the entryway signs and street signs to provide consistency and unity.*
- c. *Build strong recognition and familiarity between El Monte residents and the Downtown.*
- d. *Incorporate the Downtown logo, shown in Figure 3-13, into public signage where appropriate.*
- e. *Create a Downtown brand that becomes synonymous with a place that is safe, healthy, fun, reliable, and provides services that people value.*



Example Wayfinding Signage



Figure 3-13 Downtown Logo



Figure 3-14 Horizontal Entryway Signage

- f. Provide functional safety, transportation, and traffic information and use universal graphic symbols to effectively convey complex information.
- g. Include common directional signs with directional arrows and labeling as part of the signage program to denote key shopping areas, public vehicular and bicycle parking, and civic buildings, freeway access, and unique facilities and services.
- h. Orient directional signs to vehicular traffic. Selected signs should be lit, landscaped, and placed permanently adjacent to roadsides at locations along Valley Mall/Main Street.

3.7.2 PUBLIC SIGNAGE TYPES

Wayfinding signage is a key component in facilitating the movement of pedestrians, bicyclists, and vehicles throughout the Specific Plan area. Signage should be easy to read and visually attractive, and should enhance the form, character, and identity of Downtown El Monte. Wayfinding signs should be placed along roadsides at key locations or within landscaped areas. A wayfinding signage program should include the following signage types:

- a. *Entryway (Figures 3-14, 3-15, and 3-16)* - Entryway elements welcome visitors into the Downtown area and set the tone for improved streetscapes and overall character. Entryway signs configurations might vary, depending on site conditions, constraints, and opportunities. Signs should incorporate themed elements, fonts, and/or logos, be of high-quality materials, timeless, be reflective of the area’s character, and incorporate useful text and lighting. New entryway signage will be instrumental in providing a sense of arrival and transition into Downtown El Monte, specifically along Valley Boulevard and Santa Anita Avenue. Monument signs should be installed at main entryways to the Specific Plan area where appropriate and archways should be installed over the streets at entrances to the Downtown.

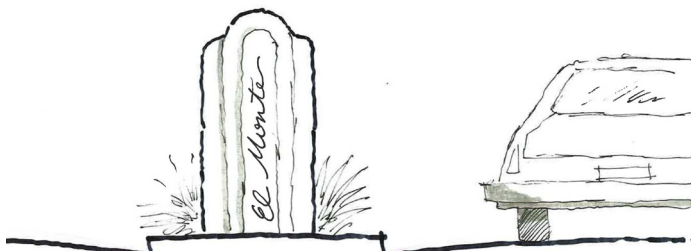


Figure 3-15 Entryway Median Signage

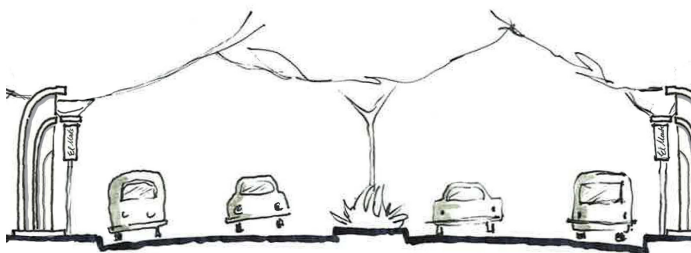


Figure 3-16 Vertical Entryway Signage

- b. *Directional Signage (Figure 3-17)* - Directional signs should be located to help direct vehicles to specific attractions. Directional signs should include direction arrows and labeling to denote important services and destinations. Vehicular directional signs should generally follow the Federal Highway Administration Manual on Uniform Traffic Control Devices (MUTCD) guidelines for design, organization, type fonts, sizes, contrast and reflectivity, but may be mounted or customized in ways which can help to reinforce the City identity established at entryways.
- c. *Information Signage (Figure 3-18)* - Additional signs that should be incorporated into a sign program include interpretive signs to highlight key buildings with historical or architectural significance, maps with pedestrian directories, public parking identification signs, Metro Bike Hub location, and facility identification signs. They are smaller in scale to direct pedestrians to specific attractions. When pedestrians can easily find their way through a space, there is an associated level of comfort and security, which is very likely to translate into a well-used space that El Monte residents take pride in.
- d. *Special Event Signs (Figure 3-19)* - Non-electronic special events signs are encouraged to complement other public signs located throughout the Downtown. Special event signs may span across the street. These temporary signs should be located at the entry to Valley Mall/Main Street at Santa Anita Avenue, along Valley Boulevard in key locations and at the south entrance of Valley Mall/Main Street at Ramona Boulevard.

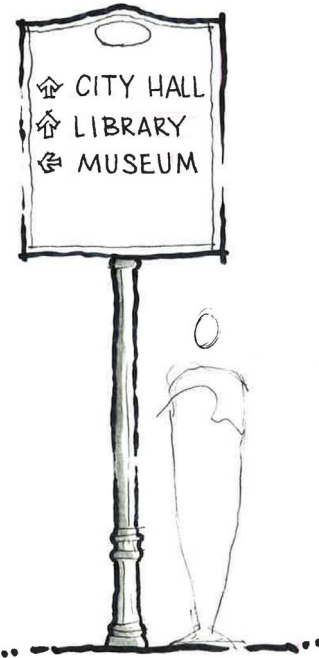


Figure 3-17 Directional Signage

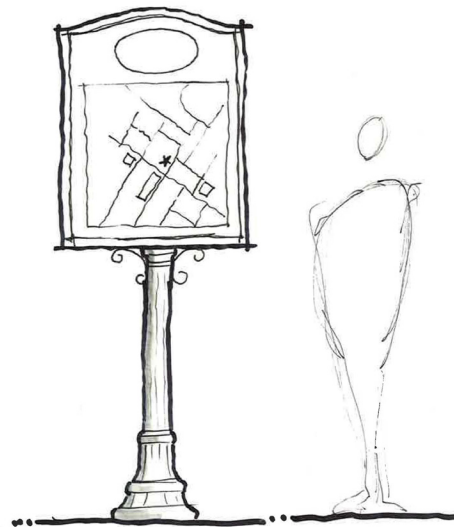


Figure 3-18 Information Signage



Figure 3-19 Special Events Signage

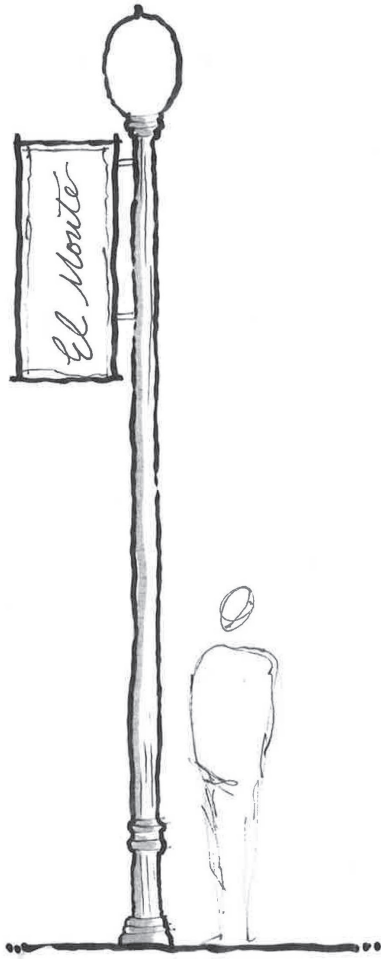


Figure 3-20 Banner Signage

- e. *Banners (Figure 3-20)* - Banners provide an opportunity to brand Downtown, advertise local activities and special events, provide directional signage, or can be decorative and celebratory. These banners may be pole mounted and should include the El Monte logo, and should be designed to complement other street signs. Alternatively, banners may include designs by local artists.

3.7.3 SUB-AREA SIGN PROGRAMS

As sub-areas within the Specific Plan area develop and become recognizable, sub-area-specific identities may be developed and incorporated into sign programs. These sub-areas may be the same or different than the land use sub-areas identified in this Specific Plan. One scheme which might be considered is to develop an image, or brand, representing each sub-area within the City or Downtown, all fitting together to support the larger Downtown identity (*Figure 3-21*).

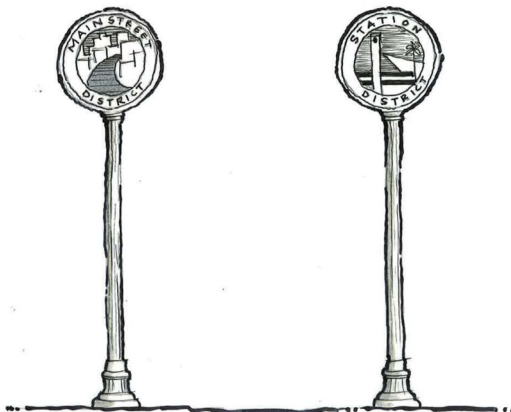


Figure 3-21 Sub-Area Signage

3.7.4 MOBILE WAYFINDING

Many El Monte visitors are likely to be carrying a smart phone or other mobile device as they visit the Downtown. This creates a unique opportunity to develop a mobile application specifically for the City. As the City of El Monte moves forward with Specific Plan implementation, this application could provide progress updates, public meeting notices, and information on how to get involved and who to contact with questions.

The application could include the following menu items:

- a. *Public Transit Routes and locations of bus stops/ shelters and stations.*
- b. *Bicycle Facilities - locations of lockers, bike share system, bike racks, and bike path routes.*
- c. *Parking Facilities - locations of public parking lots/ structures integrated with smartpark capabilities.*
- d. *Event Calendar - including any public events being held in Downtown El Monte and/or its facilities (sporting events, music, community meetings, etc.).*
- e. *Food Carts/Eateries - what type of food is available, hours of operation, location of eateries and food carts.*
- f. *Shopping - list of retail shops, hours of operation and potential to provide direct links to individual websites.*
- g. *General directory, including location of public rest rooms, drinking fountains, park amenities and hours of operation, map of Downtown.*



Mobile application for the City.

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DOWNTOWN
MAIN STREET

Chapter 4
**INFRASTRUCTURE AND PUBLIC
FACILITIES**



Chapter 4 - Infrastructure and Public Facilities

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4. INFRASTRUCTURE AND PUBLIC FACILITIES

4.1 INTRODUCTION

Public infrastructure and public services are essential to the current and future success of the Specific Plan Area. Infrastructure transports and conveys water, wastewater, stormwater, solid waste, natural gas, and electricity in support of uses within the Downtown area. Public services provide police and fire protection, education, and recreation for its residents.

4.2 WATER SYSTEM

4.2.1 EXISTING CONDITIONS

Water supply for the Plan Area is provided by the City of El Monte Water Department, which currently owns and operates the water system in the area. The City operates six production wells throughout El Monte, including two within the Plan Area, to extract water from the Main San Gabriel Groundwater Basin. The City Water Department's system includes 3,460 active service connections, which supply 18 percent of the El Monte's total land area and approximately 22,700 of its residents.

According to the 2010 Urban Water Management Plan for the City of El Monte, the City Water Department does not import water nor is it connected to a transmission pipeline of any water wholesaler. The water supply from the six deep wells has a total capacity of about 9,500 gallons per minute (gpm) or about 14,000 acre feet per year. In addition, the system utilizes a 200,000-gallon elevated tank and a one million-gallon ground level tank that provide water storage and serve the water demand. Potable water is delivered through 42 miles of pipelines, reservoirs, booster pumps, water wells, disinfection facilities, carbon filters, and emergency connections with neighboring water purveyors. Based on the City Water Department's 2014 Water Quality Report, the water quality meets or surpasses all Federal and State drinking water standards.

Water distribution within the Plan Area consists of water lines ranging from 2 to 12-inch diameter pipes that are

located under the majority of streets. The following is a list of existing water lines within the Plan Area:

Santa Anita Avenue

Existing 10" steel pipe on the east side of the street extending from the southern portion of the Plan Area at Ramona Boulevard that extends north and transitions into a 12" transit pipe just south of Valley Mall and continues north beyond the Plan Area's northern boundary.

Granada Avenue

Existing 6" steel pipe on the west side of the street extending from the southern portion of the Plan Area at Ramona Boulevard that extends north and terminates approximately 500 feet from Ramona Boulevard. There is also a 6" steel pipe on the west side of the street that extends south from Valley Mall and terminates at approximately 200 feet. A 4" steel pipe also exists along the west side of the street beginning at Valley Mall and extends north to Valley Boulevard where it terminates and connects to a 4" steel pipe that runs east along Valley Boulevard.

Lexington Avenue

Existing 6" steel pipe on the west side of the street extending from the southern portion of the Plan Area at Ramona Boulevard that extends north and terminates and connects to a 4" steel pipe running east-west along the northern side of Valley Mall.

Monterey Avenue

Existing 4" steel pipe on the west side of the street extending south from the 8" transit pipe along Railroad Street (transitioning to an 8" pipe at Valley Boulevard) and terminating/connecting to the 4" steel pipe at Valley Mall.

El Monte Avenue

Existing 4" steel pipe on the west side of the street extending south from the 8" transit pipe along Railroad Street and terminating/connecting to the 4" steel pipe at Valley Mall.

Center Avenue

Existing 6" steel pipe on the west side of the street starting at approximately 50 feet north of Valley Boulevard and extending south and terminating/ connecting to the 4" steel pipe at Valley Mall.

Tyler Avenue

Existing 8" cast iron pipe in the center of the street extending from the northern portion of the Plan Area at the railroad tracks and extends south and terminates at the 4" steel pipe along Valley Mall. At Valley Mall it transitions into a 10" transit pipe and extends south and terminates at the 10" east-west steel pipe at Iris Lane. There is also an 8" steel pipe along the east side of the street that begins at the 10" transit pipe along Valley Mall and continues south beyond Ramona Boulevard.

Wiggins Avenue

Existing 2" transit pipe in the center of the street beginning at the northern boundary of the Plan Area (railroad tracks) and extending south and terminating/ connecting to an 8" transit pipe along the eastern portion of Valley Blvd.

Ramona Boulevard

Existing 6" cast iron pipe beginning from Santa Anita Avenue at the western boundary of the Plan Area extending east along the south side of the street and terminating at the eastern boundary of the Plan Area at Valley Boulevard. The pipe transitions from a 6" cast iron pipe to a 6" transit pipe at Lexington Avenue and to a 10" transit pipe at Tyler Avenue.

Iris Lane

Existing 10" transit pipe along the north side of the street extending from the 6" steel pipe at Lexington Avenue and terminating/connecting to the 10" transit pipe at Valley Mall.

Valley Mall

Existing 8" transit pipe along the south side of the street from Santa Anita Avenue to Ramona Boulevard and beyond. There is a secondary 4" steel pipe located along the northern side of the street stretching from beyond Santa Avenue in the west and terminating at Tyler Avenue.

Valley Boulevard

Existing 10" steel pipe located on the south side of the street extending from the western end of the Plan Area from Santa Anita Avenue and terminates at Tyler Avenue. There is a secondary 4" steel pipe located on the north side of the street stretching from Granada Avenue to Monterey Avenue. There is also an existing 8" transit pipe located on the north side of the street extending from Tyler Avenue and terminating/connecting to the existing 10" transit pipe at Ramona Boulevard.

Court Adair

Existing 2" steel pipe stretching along the entire length of the street and terminates/connects to the 4" steel pipe at Monterey Avenue.

King Court

Existing 2" steel pipe stretching along the entire length of the street and terminates/connects to the 4" steel pipe at Monterey Avenue.

Railroad Street

Existing 8" transit pipe located on the south side of the street extending from El Monte Avenue in the west and terminating at Tyler Avenue to the east.

Center parking area bounded by Valley Mall to the north, Iris Lane to the South, Lexington Avenue to the west and Tyler Street to the east.

Existing 2" steel pipe on Palm Court beginning at Valley Mall and extending south (approximately 400 feet) to the northern edge of the school district property. There is also a 4" steel pipe beginning at Valley Mall extending south through the pedestrian walkway, transitioning to a 4" transit pipe in a southeast direction along the vehicular alleyway to Cleminson Street and extending directly south through the Las Flores Drive residential planned unit development, terminating at the 10" transit pipe at Iris Lane. An additional cast iron pipe (size unknown) extends from the 6" steel pipe at Lexington Avenue east to the 2" steel pipe at the terminus of the Palm Court where it connects to the existing 2" steel pipe.

4.2.2 WATER SYSTEM IMPROVEMENTS

The implementation of the Specific Plan at buildout (2035) is anticipated to increase the Plan Area’s housing stock by 2,200 units and commercial uses by 500,000 square feet of building area. It is estimated that this will increase water demand by 513,375 gallons per day. Based on the availability of future water supply, the City Water Department will be able to accommodate the projected water demand. However, City Public Works Department has identified areas of aging water pipes and facilities that require improvements to distribute the water to its customers. These water system improvements are included in the City’s 2015/2016 Capital Improvement Program (CIP), which have been funded and approved by the City Council. The necessary improvements to the

existing water system deficiencies in the Plan Area are identified below and illustrated in Figure 4-1.

- Complete the installation of water meters (citywide)
- Construct Well No. 3 and blend with Well No. 2A
- Replace water main at 3500 block of Granada Avenue
- Replace 10- and 12-inch water main on Valley Boulevard from Santa Anita Avenue to Johnson Avenue
- Replace 8-inch water main in the Valley Mall east of Granada Avenue
- Replace 10-inch water main on Tyler Avenue from Brockway Street to Ramona Boulevard

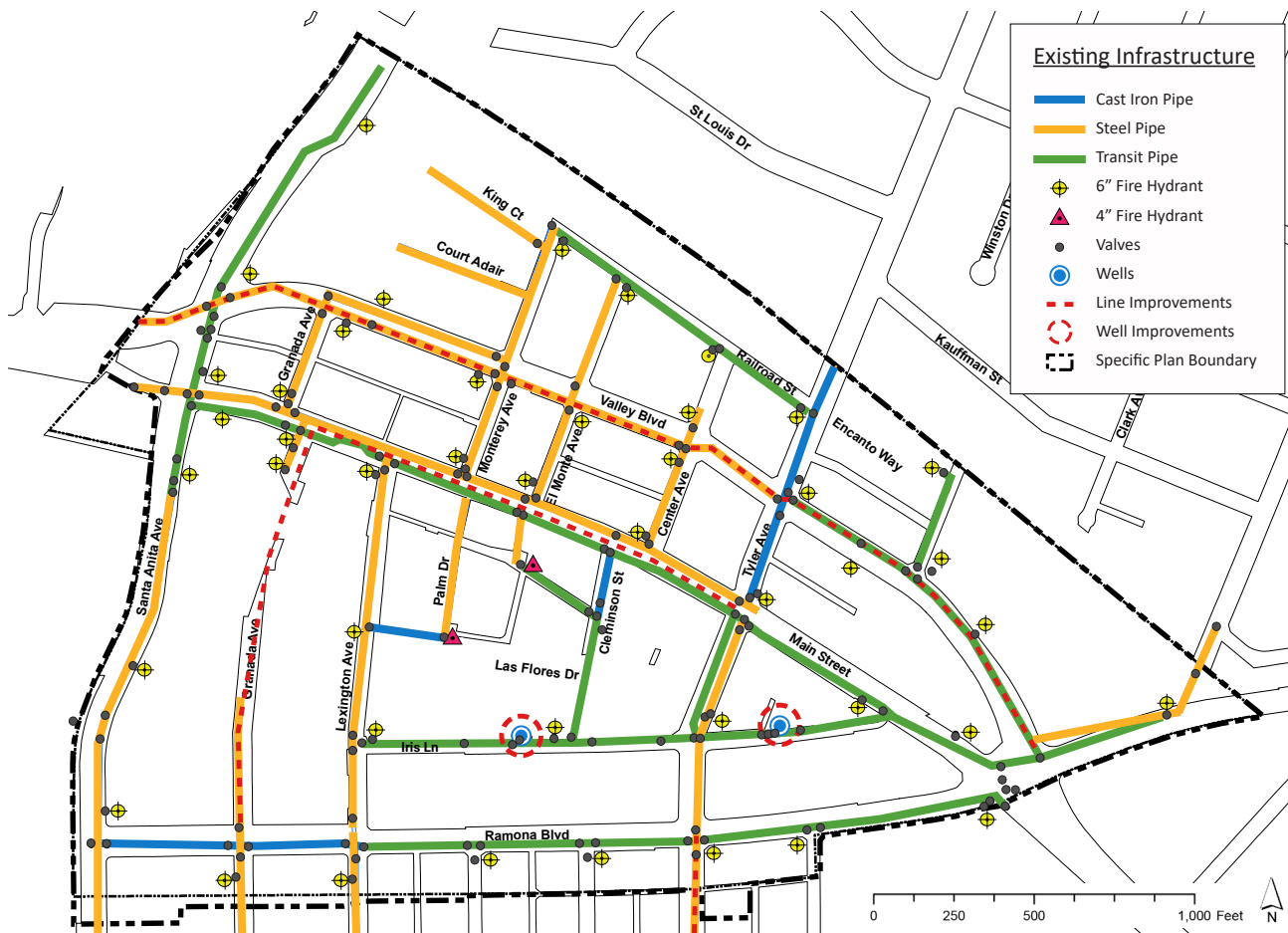


Figure 4-1 Water System Improvements

These water system improvements address the current demand, additional replacements and upgrades of the water distribution system may be necessary to accommodate water demand and fire-flow requirements as specific projects are approved in the future. Therefore, the Specific Plan includes the following requirements:

- All new development projects shall submit a water system analysis to verify the necessary infrastructure improvements needed to serve the project. The general requirements for this analysis include: 1) a minimum of 40 pounds per square inch (psi) delivered during peak hour demand (PHD); and 2) a minimum of 20 psi delivered during maximum day demand (MDD) plus fire flow (FF). The project developer will be required to fund the cost of the improvements
- All new development projects shall comply with applicable fire and life safety standards and code requirements established by the Los Angeles County Fire Department, including, but not limited to, fire hydrant flow, hydrants spacing, and water supply connections, which must be adequately sized to the satisfaction of the County Fire Department.
- The City Water Department shall prepare the Urban Water Management Plan update to include the level of development proposed by this Specific Plan in the planning of future water supply/demand.
- The City Water Department shall implement water conservation Best Management Practices (BMPs) identified in the Urban Water Management Plan update.

4.3 WASTEWATER SYSTEM

4.3.1 EXISTING CONDITIONS

The sanitary sewer system within the Plan Area is owned, operated, and maintained by the City of El Monte Public Works Department, with the exception of a trunk main line at the western portion of the Plan Area that is owned and maintained by the Los Angeles County Sanitation District.

According to the City's General Plan, the City of El Monte is one of 17 jurisdictions that are signatory to the Joint Outfall Agreement. The Agreement provides for a

regional interconnected system of facilities and an inter-jurisdictional agreement to own, operate and maintain sewers, pumping plants, treatment plants, and other facilities collectively called the Joint Outfall System.

There is a comprehensive network of sewer lines in the Plan Area. The existing sewer mains in this project area are mostly 8" Vitrified Clay Pipe (VCP), with the exception of other sewer mains ranging in size from 10" to 15". The following is a list of existing wastewater lines within the Specific Plan Area:

Santa Anita Avenue

Existing 8" VCP sewer main on the center line (CL) of the street from Valley Mall to Ramona Boulevard.

Granada Avenue

Existing 8" VCP sewer main on the CL of the street from Valley Boulevard south bound to Valley Mall. There is an existing 12" VCP sewer main on the CL of the street from Valley Mall south to Ramona Boulevard.

Lexington Avenue

Existing 8" VCP sewer main beginning approximately 100' south of Valley Mall on the CL of the street south bound to Ramon Boulevard. An additional 8" VCP sewer main approximately 500 feet south of Valley Mall extends east from Lexington Ave. approximately 425 feet where it terminates between Palm Drive and Cleminson Street.

Monterey Avenue

Existing 8" VCP sewer main on the CL of the street beginning at Railroad Street south bound to Valley Boulevard. An additional VCP sewer main extends approximately 275 feet west of Monterey Ave. just north of Valley Mall along the rear alleyway between commercial retail structures and the City owned parking lot.

El Monte Avenue

Existing 8" VCP sewer main on the CL of the street beginning at Valley Boulevard south bound to Valley Mall.

Center Avenue

Existing 8" VCP sewer main on the CL of the street beginning at Railroad Street and terminating to the south at Valley Mall.

Tyler Avenue

Existing 10" VCP sewer main on the west side of the street extending from the northern portion of the Plan Area at the railroad tracks that extends south and terminates at Valley Mall. There is a secondary 8" VCP sewer main (on the east side of the street) also extending from the northern portion of the Plan Area at the railroad tracks that extends south and continues beyond Ramona Boulevard. However, at Valley Boulevard the size of the sewer main increases to 18".

Wiggins Avenue

Existing 8" VCP sewer main extending from the northern boundary at the railroad tracks south to Valley Boulevard.

Palm Drive

Existing 8" VCP sewer main beginning approximately 100 feet south of Valley Mall extending south 375 feet to its terminus.

Cleminson Street

Existing 8" VCP sewer main beginning approximately 125 feet south of Valley Mall extending south to its terminus at Iris Lane.

Ramona Boulevard

Existing VCP sewer main on the CL of the street that extends to the east (transitioning to the northern side of the street at Lexington Avenue) and beyond the eastern project area on Valley Boulevard. The VCP sewer main ranges in diameter from 15" to 10" to 8" along this route.

Iris Lane

Existing 8" VCP sewer main, on the CL of the street, beginning at Tyler Avenue and extending approximately 375 feet to the west where it terminates mid-block.

Valley Mall

Existing 8" VCP sewer main, on the CL of the street, extending from the western Plan Area boundary at Santa Anita Avenue extending east to its termination at approximately 75 feet west of Iris Lane. The VCP sewer main increases in size to 12" between Granada Avenue and Tyler Avenue.

Valley Boulevard

Three separate segments of 8" VCP sewer main, on the CL of the street, along Valley Boulevard. The first segment begins at Granada Avenue and extends east to approximately 125 feet west of Monterey Avenue. The second segment begins at El Monte Avenue and extends east to approximately 125 feet west of Center Avenue. The third segment begins approximately 100 feet east of Tyler Avenue and extends east and terminates at approximately 125 feet from Ramona Boulevard.

Court Adair

Existing 8" VCP sewer main on the CL of the street stretching along the entire length of the street and terminating/connecting to the 8" VCP sewer main at Monterey Avenue.

King Court

Existing 8" VCP sewer main on the CL of the street stretching along the entire length of the street and terminating/connecting to the 8" VCP sewer main at Monterey Avenue.

Railroad Street

Two separate segments of 8" VCP sewer main. The first segment begins at Monterey Avenue and extends to the east terminating at approximately 175 feet west of Center Avenue. The second segment begins approximately 75 feet east of Center Avenue and terminates/connects to the 8" VCP sewer main at Tyler Avenue.

4.3.2 WASTEWATER SYSTEM IMPROVEMENTS

The Specific Plan will facilitate the development of new residential units and commercial building area. The development growth would generate an estimated 410,700 gallons per day of wastewater that would flow through the existing sewer system. The additional wastewater generated by the implementation of the Specific Plan can be adequately treated at the Joint Water Pollution Control Plant. The additional wastewater would represent only 0.41 percent of the remaining design capacity of the wastewater treatment facility.

At the citywide level, the City’s Public Works Department, Maintenance Division has identified the following necessary improvements, which are illustrated in Figure 4-2, to address the existing sewer system deficiencies:

- Complete the replacement of sewer main from an 8” VCP to a 12” VCP for a segment along Ramona Boulevard beginning beyond Valley Boulevard and terminating at Tyler Avenue.
- Design and construct sewer line from Johnson Avenue to Tyler Avenue.

Additional replacements and upgrades of the sewer system may be necessary as specific projects are

approved in the future. Therefore, the Specific Plan includes the following requirements.

- The City Public Works Department shall coordinate with the City Economic Development Department, Planning Division in preparing the Sewer Master Plan. The Sewer Master Plan shall include the level of development proposed by the Specific Plan in the planning of future sewer infrastructure improvements.
- The City Public Works Department shall upgrade the existing sewer system based on the Sewer Master Plan update and include these sewer upgrades in future CIPs. Specific

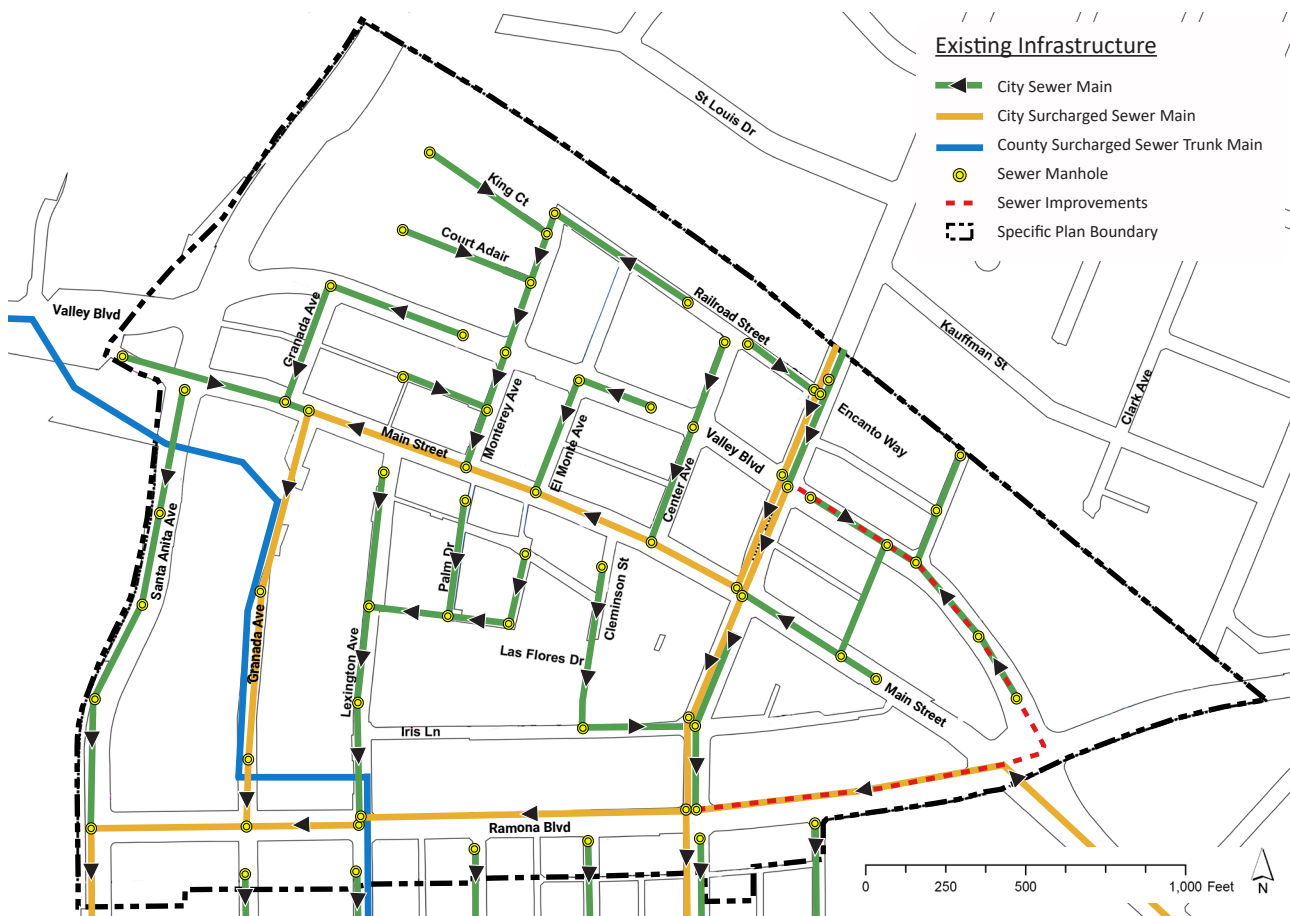


Figure 4-2 Sewer System

sewer improvements identified by the Sewer Area/Capacity Study prepared for each new development shall be funded by the project developer on a project by project basis.

4.4 STORM DRAINAGE SYSTEM

4.5.1 EXISTING CONDITIONS

The storm drain system in the City is owned and maintained by both the Los Angeles County Flood Control District (LACFCD) and the City of El Monte. The storm drain main lines within the Plan Area consist primarily of Reinforced Concrete Pipe (RCP) with varying pipe diameter sizes. The general topography of the Plan Area slopes from the northeast to the southwest. The storm water runoff collected in the Plan Area is carried through a series of interceptor storm drains to discharge points to the Rio Hondo River flood control channel. The following is a list of existing storm drainage facilities within the Plan Area:

Santa Anita Avenue

There is an existing 18" RCP storm drain pipe from the north of the Specific Plan boundary that extends south to Valley Mall where it connects to an existing 27" RCP storm drain pipe that discharges to the west at the Rio Hondo flood control channel.

Valley Mall

Existing 12" RCP storm drain beginning approximately 75 feet west of Lexington Avenue and extends west where it transitions to a 15" pipe, just west of Granada Avenue, and transitions to a 27" pipe at Santa Anita Avenue extending west to its discharge at the Rio Hondo.

There is an additional storm drain pipe (size unknown) beginning on the north side of the street on the east side of Monterey Avenue that extends across the street to the south and travels east along the south side of the street, and transitions to the south on Las Flores Drive, then continues south through the center parking and terminates/connects to the Los Angeles County storm drain at Iris Lane.

There is an additional 12" RCP storm drain pipe that begins at Tyler Avenue approximately 75 feet north of Valley Mall and extends southeast to the north side of Valley Mall traveling southeast (transitioning to a 15" pipe at Esmeralda Avenue) to its termination/connection to the Los Angeles County storm drain at Iris Lane.

Valley Boulevard

Existing 27" RCP storm drain pipe located on the south side of Valley Blvd located between Santa Anita Avenue and the Rio Hondo flood control channel and discharges in a northerly direction into the Rio Hondo.

There is also an existing storm drain pipe (size unknown) beginning approximately 75 feet north of Valley Boulevard on the west side of Tyler Avenue and extends southeast across Tyler Avenue, and continues east along the north side of Valley Boulevard, then terminates/connects to the Los Angeles County storm drain approximately 300 feet northwest of Ramona Boulevard.

Ramona Boulevard

Two existing 18" RCP storm drains on Ramona Boulevard connect/terminate to the 96" Los Angeles County storm drain. The first one begins at the northeast intersection of Lexington Avenue and extends northwest across Lexington Avenue to the west side of the street, and continues north to its terminus at the Los Angeles County storm drain on Iris Lane. The second begins at the northeast intersection of Tyler Avenue and extends northwest across Tyler Avenue to the west side of the street, and continues north to its terminus at the Los Angeles County storm drain on Iris Lane. Figure 4-3 illustrates the existing storm drainage system within the Plan Area.

4.4.2 STORM DRAINAGE SYSTEM REQUIREMENTS

The Plan Area is mostly built out with impervious surfaces with the exception of some undeveloped lots in the northwest corner of the Plan Area. Therefore, only an incremental increase in the quantity of impervious surfaces is expected. Any increase in impervious surfaces could increase the overall volume of stormwater runoff by reducing infiltration.

According to the City Public Works Department, the Plan Area has not experienced flooding during storms periods and there are currently no known deficiencies in the existing storm drain system. However, the Specific Plan includes the following requirements.

- All new development projects shall conduct an on-site hydrology study to assess stormwater runoff from the proposed development, and propose appropriate improvements to the existing storm drainage system and detention facilities.
- All new development projects shall comply with El Monte Municipal Code Section 13.20.020, which includes requirements for site design and post-

construction BMP operation and maintenance of development and redevelopment projects to comply with the city of El Monte's municipal NPDES permit currently in effect at the time of development application submittal. This will lessen the water quality impacts of development by using smart growth practices, and integrate LID design principles to mimic pre-development hydrology through infiltration, evapotranspiration and rainfall harvest and use.

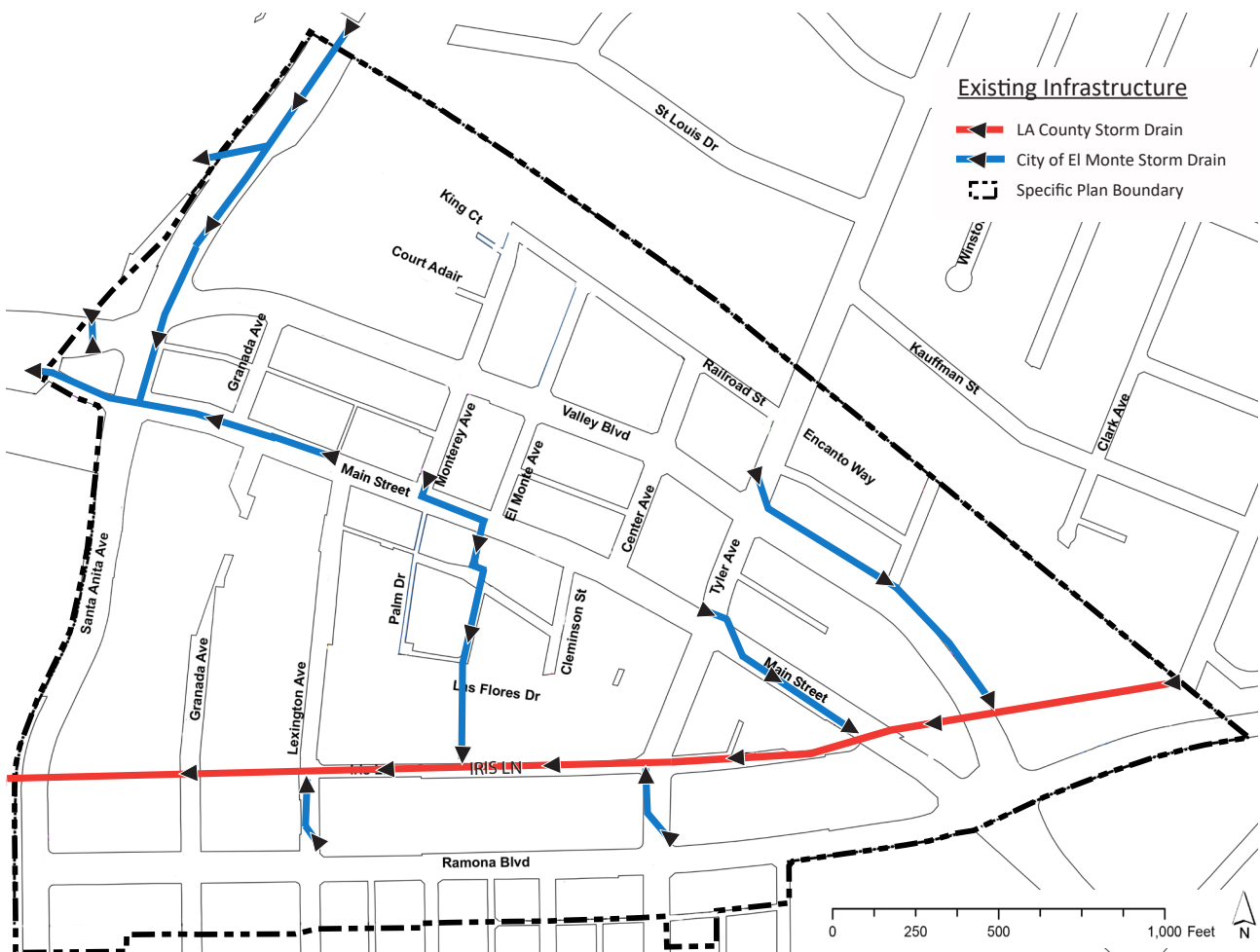


Figure 4-3 Storm Drainage System

4.5 SOLID WASTE DISPOSAL

4.5.1 EXISTING CONDITIONS

El Monte is served by three waste management companies: Valley Vista Services; American Reclamation; and Waste Management. Table 4-1 presents the percentage of disposal service by each waste management company for the total City. Valley Vista Services provides 100 percent of the Citywide curbside residential (single family) collection and recycling services, 50 percent of the collection and recycling of trash from multiple family residential (apartments, townhomes), and 50 percent of commercial collections. American Reclamation provides 50 percent of the multifamily disposal service and 25 percent of the commercial. Waste Management provides 25 percent of the commercial disposal service.

Solid waste disposal service for the Plan Area is provided primarily by Valley Vista, with the exception of the Valley Mall, which is served by American Reclamation. Valley Vista provides once per week collection of trash and recyclable waste from El Monte residents. Valley Vista also uses compressed natural gas (CNG) for its trash collection vehicles.

4.5.2 SOLID WASTE DISPOSAL REQUIREMENTS

Implementation of the Specific Plan at buildout would generate an increase of up to 9.4 tons of solid waste per day. Currently, most of the waste is disposed of at the Olinda Alpha Sanitary Landfill in Orange County and the El Sobrante Landfill in Riverside County. The maximum permitted daily disposal rate for Olinda Alpha Sanitary

Landfill is 8,000 tons of solid waste per day and 16,054 tons per day into the El Sobrante Landfill. The solid waste generated by the Specific Plan overall represents 0.12 percent of the maximum permitted daily disposal rate for Olinda Alpha and 0.06 percent for El Sobrante. Therefore, both landfills have adequate capacity to serve the Specific Plan.

The following are solid waste disposal requirements:

- Solid waste reduction and control measures shall be implemented by the City for public spaces and the project developers for private development projects.
- All new development in the City will be required to implement existing and future waste reduction programs in conformance with the City’s Source Recovery and Recycling Element (SRRE) as required by Assembly Bill (AB) 939, the California Integrated Waste Management Act of 1989.
- Any hazardous waste that is generated on-site, or is found on-site during demolition, rehabilitation, or new construction activities is the responsibility of the project developer, and shall be remediated, stored, handled, and transported to an appropriate disposal facility by a licensed hauler in accordance with local, State and Federal laws, as well as with the City’s SRRE.

4.6 ENERGY SYSTEM

4.6.1 EXISTING NATURAL GAS

The natural gas purveyor for the City of El Monte is the Southern California Gas Company, also known as Sempra Energy Utility Company. The entire City has an extensive gas facility network with comprehensive coverage. Every parcel within the Plan Area has access to natural gas. The existing gas lines within the Plan Area range from 1/2” to 20” in diameter. The primary natural gas line locations are illustrated in Figure 4-4 and discussed in the following list:

TABLE 4-1 SOLID WASTE DISPOSAL SERVICES

Disposal Companies	Residential	Multifamily	Commercial
Valley Vista	100%	50%	50%
American Reclamation	--	50%	25%
Waste Management	--	--	25%

Santa Anita Avenue

Existing 10", 6", and 3" gas main lines traversing most of Ramona Boulevard. Some portions of the street include a 2" line, particularly near the southwest portion of the Specific Plan Area.

Ramona Boulevard

There are existing 10", 6", and 3" gas main lines traversing most of Ramona Boulevard. Some portion of the street include a 2" line, particularly near the southwest portion of the Specific Plan area.

Granada Avenue

Existing 6" gas main line traversing most of Granada Avenue. The portion of Granada Avenue north of Main Street is comprised of a 2" gas main line.

Lexington Avenue

Existing 2" gas main line traversing Lexington Avenue.

Monterey Avenue

Existing 2" and 3" gas main lines traversing Monterey Avenue.

El Monte Avenue

Existing 2" gas main line traversing El Monte Avenue.

Center Avenue

Existing 2" gas main line traversing Center Avenue.

Tyler Avenue

Existing 2" gas main line that travels north from the



Figure 4-4 Primary Natural Gas Line Locations

intersection of Tyler Avenue and Iris Lane. At the intersection of Tyler Avenue and Valley Boulevard, the gas main line increases in size to a 4" gas main line and then again to a 20" gas main line.

Wiggins Avenue

Existing 2" gas main line that traverses Wiggins Avenue.

Iris Lane

Existing 2" gas main line that traverses Iris Lane.

Las Flores Drive

Existing 2" gas main line that traverses Las Flores Drive.

Main Street

Existing 4" gas main line that traverses Main Street from the Specific Plan Boundary terminating at Ramona Boulevard. An existing 2" gas main line begins at El Monte Executive Plaza and increases in size to a 4" gas main line before transition down to 2" at the El Monte Avenue intersection and then increasing to 3" at Monterey Avenue before becoming a 6" gas main line ending before Santa Anita Avenue.

Valley Boulevard

Existing 2" gas main line that begins along Valley Boulevard south of the Post Office that transitions to a 6" gas main line, terminating at the intersection of Tyler Avenue. A 4" gas main line begins again mid-block along Valley Boulevard between, Central Avenue and El Monte Avenue, before terminating at El Monte Avenue again. A 4" gas main line begins again at Monterey Avenue and decreases mid-block to a 3" gas main line and then to a 2" gas main line before the Granada Avenue intersection.

Other

A number of additional gas main lines traverse through and out of the Specific Plan area. These gas main lines run along back alleys, through parking lots, or through individual properties.

4.6.2 NATURAL GAS REQUIREMENTS

The following requirement shall ensure that adequate natural gas service is available in the Plan Area:

- The City Public Works Department shall coordinate with the Southern California Gas Company for system expansion needs associated with Specific Plan buildout.

4.6.3 EXISTING ELECTRICAL SYSTEM

The Southern California Edison Company is the electricity purveyor for the City of El Monte. The existing electrical network within the Plan Area is a combination of overhead power poles carrying low voltage conduits along with telecommunication lines and cable TV. Most of the overhead electrical lines are located along minor side streets, alleys, and within interior and rear parking areas. Figure 4-5 illustrates the existing overhead electrical lines, and the following is a list of existing overhead electrical facilities:

King Court

Existing overhead electrical lines on the north side of the street from Monterey Avenue to its terminus.

Court Adair

Existing overhead electrical lines on the north side of the street from Monterey Avenue to its terminus.

Monterey Avenue

Existing overhead electrical lines beginning at Valley Boulevard and extending north to Railroad Street.

El Monte Avenue

Existing overhead electrical lines beginning at Valley Boulevard and extending north to Railroad Street.

Center Avenue

Existing overhead electrical lines beginning at Valley Boulevard. and extending north to Railroad Street.

Railroad Street

Existing overhead electrical lines located on the north side of the street beginning from El Monte Avenue and extending east and terminating approximately 175 feet west of Tyler Avenue.

Alleyway between Valley Blvd. and Encanto Way

Existing overhead power lines beginning at approximately 100 feet from Tyler Avenue and extending east to Wiggins Avenue.

Wiggins Avenue

Existing overhead power lines located on the west side of the street beginning immediately north of Valley Boulevard extending north to the railroad right-of-way.

Northern Specific Plan boundary at the Railroad right-of-way

Existing overhead power lines beginning at Wiggins Avenue extending east to Ramona Boulevard.

In and around the Valley Mall Area

A series of overhead power lines scattered throughout the Valley Mall area within and around the rear parking areas. A series exists behind the northern commercial/retail buildings along Valley Mall, beginning at Santa Anita Avenue and extending east along the vehicular alley, to approximately 200 feet east of Tyler Avenue. Another portion of overhead power lines exists along Granada Avenue, beginning at Valley Mall extending south to overhead power lines within the rear vehicular alley, and transitioning east along the rear of the commercial/retail structures, and extending southeast to Palm Drive.

A series of overhead power lines extends south from this series to its terminus at the northern edge of an existing

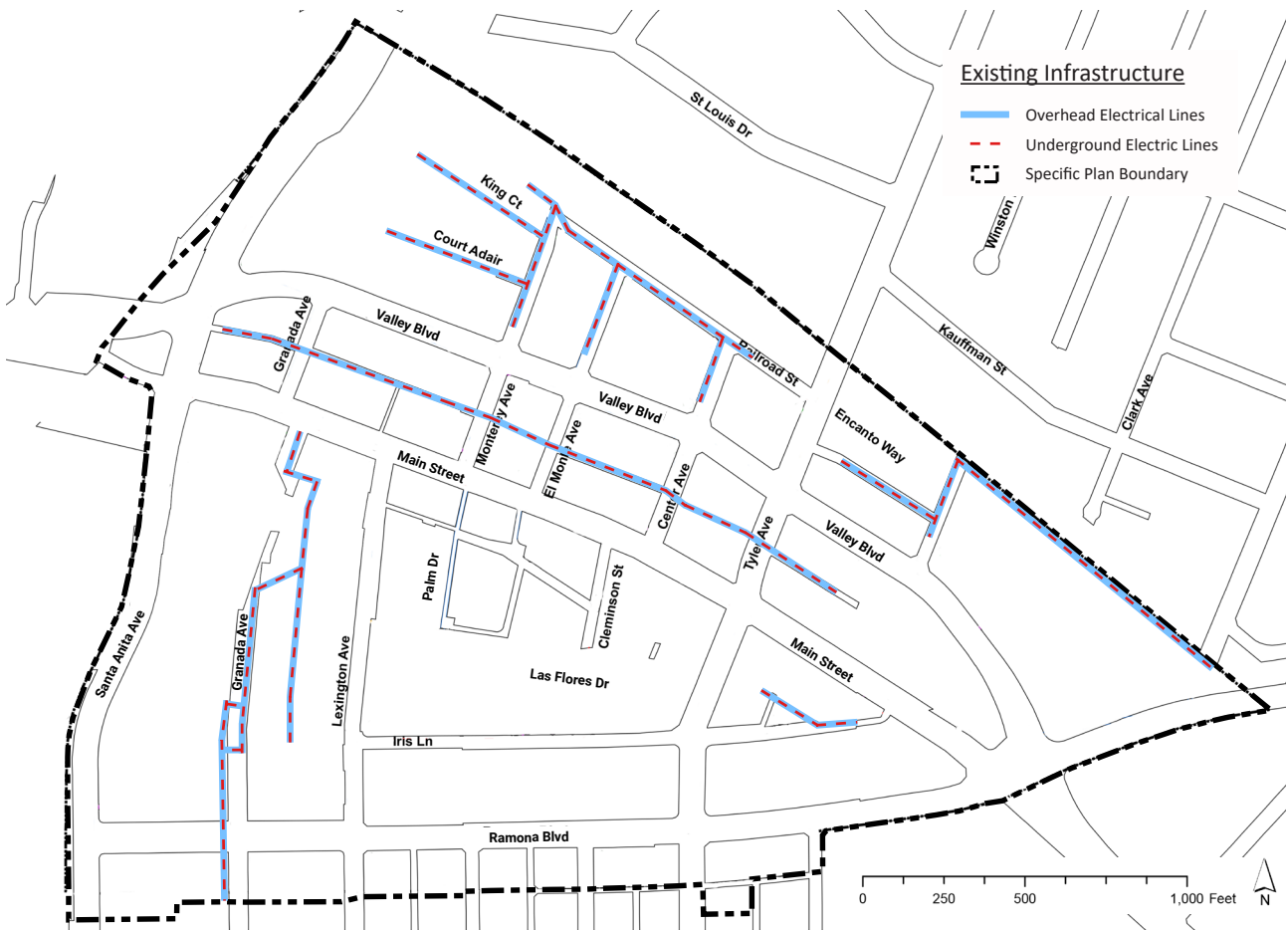


Figure 4-5 Existing Overhead Electrical Lines

parking facing Ramona Boulevard. An additional series of overhead power lines exist behind the commercial/retail structures located east of Tyler Avenue extending to the terminus of Iris Lane.

Granada Avenue

Existing overhead power lines located along the east side of the street, extending from beyond the southern boundary of the project area, north to the southern boundary of the centrally located City owned parking lot.

4.6.4 ELECTRICAL SYSTEM IMPROVEMENTS

The following requirement shall ensure that adequate electrical service is available in the Plan Area:

- The City Public Works Department shall coordinate with Southern California Edison to underground existing overhead electricity lines.
- For new development, undergrounding of utilities shall be required where a nexus is identified.

4.7 POLICE PROTECTION SERVICES

4.7.1 EXISTING CONDITIONS

The City of El Monte Police Department provides police protection services in the City. The main police station is located at 11333 Valley Boulevard and includes a temporary jail facility. This station is located adjacent to City Hall and is less than one-quarter mile east of the Plan Area. Figure 4-6 illustrates the location of the police station as well as other public facilities. The second station, primarily a community relations office, is located at 10503 Valley Boulevard, which is approximately one-tenth of a mile west of the Plan Area. The Police Department also commands an air-support unit office at the San Gabriel Valley Airport, where two helicopters can be dispatched to assist police operations in the City. The cities of Montebello, Irwindale, and Baldwin Park contract with the City of El Monte to receive air support for police operations.

The Police Department enforces all local, state, and federal laws, performs investigations and makes arrests, administers emergency medical treatment, and responds

to City emergencies. According to the El Monte General Plan, the total number of Police Department staff include 127 police officers, 46 civilian staff, and four K-9 units. The City employs about 1.1 police officers per 1,000 residents.

4.7.2 POLICE DEPARTMENT REQUIREMENTS

Implementation of the Specific Plan would add up to 2,200 residential units and 500,000 square feet of commercial uses at buildout in the year 2035. The potential development growth could increase the population of the Plan Area by 8,465 residents. Based on current staffing levels necessary to serve the residents of El Monte, the Police Department station may need additional officers to maintain adequate coverage for increased population resulting from buildout of the Specific Plan. Based on a current ratio of 1.1 officers to 1,000 residents, there is the potential need for nine additional officers resulting from buildout of the Specific Plan.

The potential demand for additional personnel, equipment, and operational costs generated by the proposed Specific Plan would be funded and offset through the increased tax revenue generated from the specific development projects allowed under the Specific Plan.

The following requirement shall ensure that adequate police protection services are available in the Plan Area:

- As condition of approval, all large developments within the City shall address the impacts of development on police and fire protection services. The City may initiate a study for additional new public safety services or public safety service facilities for the benefit of supporting future multi-family developments within the City. The information from sources such as the study, but not limited to the study, may be used by the City to initiate proceedings to form a community facilities district (CFD) to provide a source of funding to pay for any such additional level of public safety services or new public safety service facilities for the benefit of the project.

4.8 FIRE PROTECTION SERVICES

4.8.1 EXISTING CONDITIONS

The City of El Monte maintains a contractual agreement with the Los Angeles County Fire Department (LACoFD) to provide fire protection and emergency vehicle services for the City. There are four fire stations located in the City of El Monte, which are part of Division IX, Battalion 10 of LACoFD. The four fire stations within the City limits include:

1. Fire Station 166, 3615 Santa Anita Avenue, El Monte
2. Fire Station 167, 11567 Bryant Road, El Monte
3. Fire Station 168, 3207 Cogswell Road, El Monte
4. Fire Station 169, 5112 North Peck Road, El Monte

Fire Station No. 166 is the nearest fire station to the Plan Area and is located adjacent to the western boundaries of the Plan Area in the vicinity of the intersection of Valley Mall and Santa Anita Avenue. Currently, Fire Station No. 166 has four fire personnel on duty per shift or 12 personnel during a full day (three shifts). Figure 4-6 below shows the locations the fire stations within the City.

The adequacy of fire protection service is measured through response times, which refers to the time it takes from receipt of a call to arrival at an emergency site. The LACoFD uses national guidelines of a five-

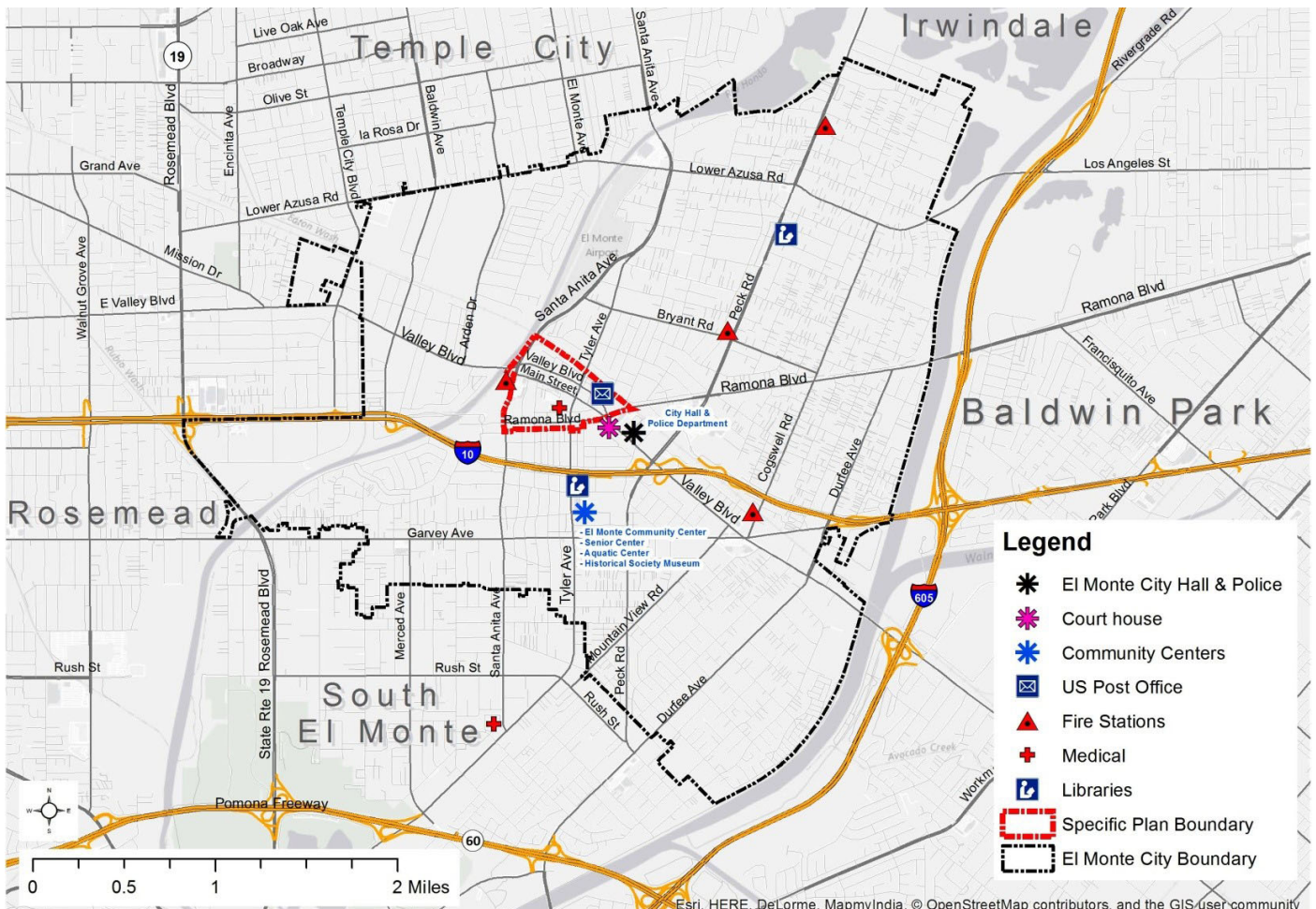


Figure 4-6 Public Facilities

minute response time for the first-arriving unit for fire and emergency medical responses. The LACoFD indicates that the average response time from Fire Station No. 166 to any part of the Plan Area is less than five minutes. In the event of a large-scale emergency in the City of El Monte, fire service from other stations within Battalion 10 would respond from the Cities of Rosemead, Temple City, and South El Monte.

4.8.2 FIRE PROTECTION REQUIREMENTS

New residential and commercial development resulting from the implementation of the Specific Plan is expected to increase demand for fire and emergency incidents services. Fire Station No. 166 will respond to most of the fire calls from the Plan Area. It is anticipated that based on the potential level of housing and commercial growth in the Plan Area, Fire Station No. 166 would require an increase of two to three additional personnel per shift and additional fire equipment. The need for additional staff and equipment, which will also increase operational costs, may necessitate the expansion of the existing fire station.

The potential costs associated with the potential expansion of staffing and facilities would be funded and offset through the increased tax revenue generated from the Specific Plan development. The following are requirements to ensure adequate fire protection services within the Plan Area:

- The Los Angeles County Fire Department shall review all development projects for compliance with applicable fire and life safety standards and code requirements established by the County. These safety standards and requirements include fire hydrant flows and spacing, adequate fire lane turning radii, adequate access and design, and adequately sized water supply connections. All Fire Department requirements shall be incorporated into the development project's site plan. All necessary safety improvements shall be funded by the project developer.
- As a condition of approval, all large developments within the City shall address the impacts of the development on police and fire protection services.

The City may initiate a study for additional new public safety services or public safety service facilities for the benefit of supporting future multi-family developments within the City. The information from sources such as the study, but not limited to the study, may be used by the City to initiate proceedings to form a community facilities district (CFD) to provide a source of funding to pay for any such additional level of public safety services or new public safety service facilities for the benefit of the project.

4.9 PARKS AND RECREATION

4.9.1 EXISTING CONDITIONS

There are 11 parks operated by the City of El Monte Park, Recreation and Community Services Department that total 51.25 acres of park space within the City. None of the parks are located within the Plan Area, but a few are in close proximity including Arceo Park on Tyler Avenue which includes summer programming.

The Recreation and Community Services Department also maintains the El Monte Community Center at 3130 North Tyler Avenue across from Arceo Park. This facility serves the entire City and contains a large meeting auditorium (Grace T. Black Auditorium), the El Monte Historical Society, the Jack Crippen Senior Center, recreational rooms, administrative offices, and outdoor courtyards. The City has an Aquatic Center across from the Community Center providing year-around swimming recreation in an indoor pool. The City has a sports program offering basketball, baseball and golf activities. There are also a number of recreational classes available to residents all year round. Figure 4-7 maps the City parks and recreational facilities. The largest City park is Pioneer Park covering 11.2 acres. Other parks and facilities in the City include, Gibson Mariposa Park at 4.3 acres, Mountain View Park at 10.1 acres, Rio Vista Park at 1.5 acres, and Zamora Park at 5.2 acres. These parks are primarily neighborhood serving parks.

The Plan Area is also adjacent to the regional "Emerald Necklace," which is a vision for a series of parklands in the region along the Rio Hondo and San Gabriel Rivers

framing the City of El Monte and helping to enrich the natural environment along the 17-mile loop connecting several cities along these rivers. Regional parks that serve the El Monte population include the Santa Fe Dam Recreational Area in the City of Irwindale and the Whittier Narrows Recreation Area adjacent to the City of South El Monte.

4.9.2 PARKS AND RECREATIONAL REQUIREMENTS

Based on a standard of one acre of parkland to every 1,000 residents, the projected addition of up to 8,465 new residents would translate to a need of approximately eight acres of new parkland. To address new development impacts, Municipal Code Section 16.34.030 (Parkland Dedication) requires dedication of land, payment of fees

in-lieu thereof, or a payment and dedication combination for park and/or recreational purposes, including open space purposes. The standards for land dedication or in-lieu fees are established in City Council Ordinance No. 2663.

4.10 SCHOOLS

4.10.1 EXISTING CONDITIONS

The El Monte community is served by three public school districts, 35 public schools and 10 private schools. The Plan Area is served by the El Monte City School District and the El Monte Union High School District. Figure 4-7 below illustrates the location of the schools in the City.

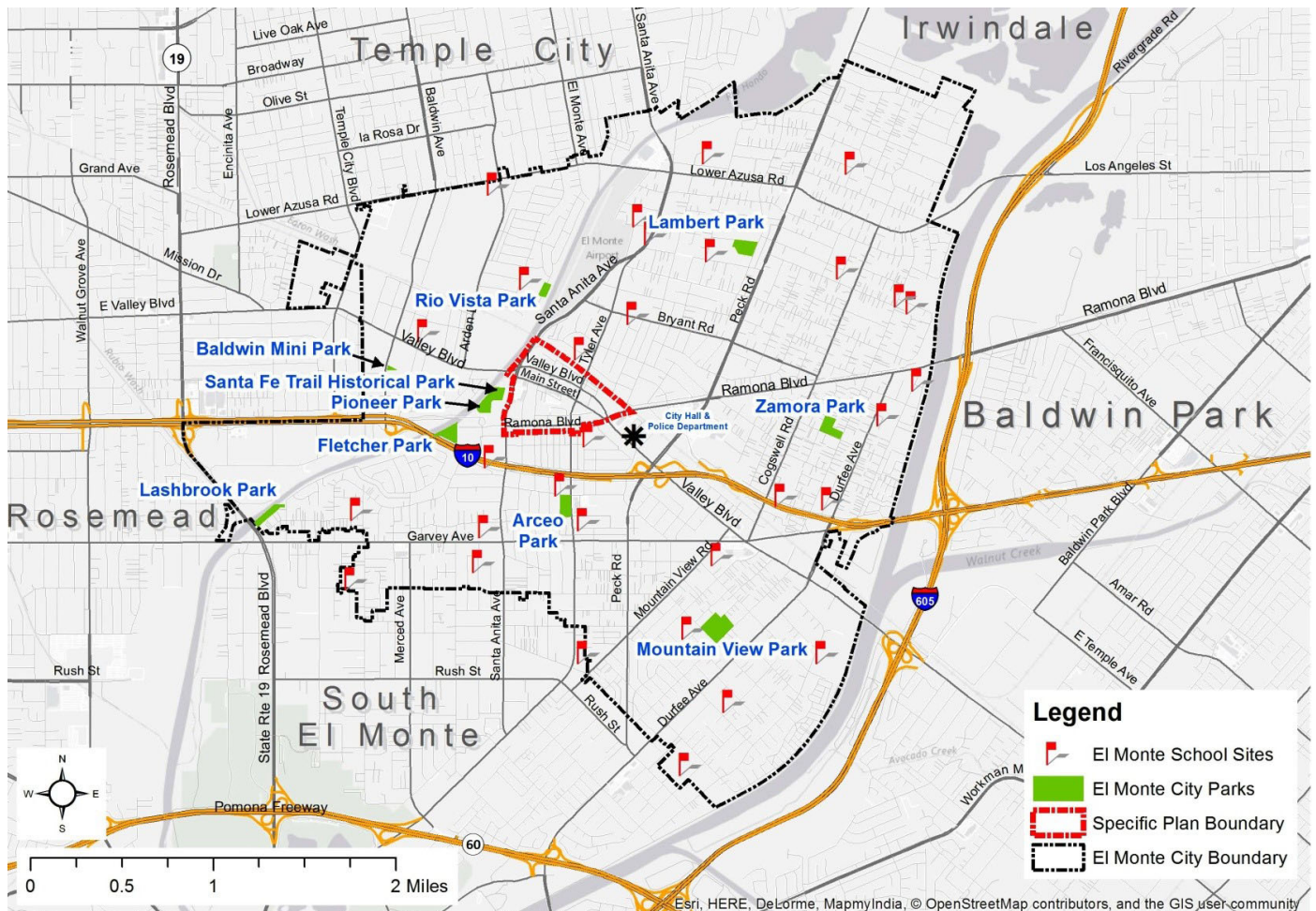


Figure 4-7 Parks and Recreational Facilities

There are no K-12 schools located within the Plan Area but the Rosemead-El Monte Adult School has classrooms and administrative offices in the Plan Area at 10807 Ramona Boulevard. The El Monte City School District administrative offices are also located with the Plan Area at 3540 North Lexington Avenue. Both the El Monte-Rosemead Adult Education Center and Rio Hondo College serve the adult populations in and around the Plan Area.

Based on demographic projections and reports prepared by the California Department of Education, for the 2000 to 2014 period, the K-12 public school enrollment in El Monte has been declining steadily from 24,559 students in 2000 to 19,807 students in 2014. Similarly, K-6 and 7-9 grades have also experienced declining enrollment. Increasing enrollment in grades 10-12 was seen in the 2000 to 2014 period increasing from 4,646 students in 2000 to 4,799 students in 2014. These enrollment figures are reflective of the overall population growth in the City which has declined slightly from 2000 to 2014. Overall population decline is primarily due to declines in the age 5-20 group. The City has experienced population increases in the age 55-65 plus groups.

4.10.2 SCHOOL REQUIREMENTS

Implementation of the Specific Plan would add up to 2,054 new households within the Plan Area and a potential corresponding addition of 1,467 K-12 students to current student rolls. The school districts will incur additional costs to accommodate the new students. These costs shall be funded through developer fees allowed under Government Code Section 65995, which authorizes school districts to collect developer fees at a maximum of \$3.36 per square foot for residential construction and \$0.54 for commercial/industrial construction (Level I fees). Level I fees are adjusted every two years according to the inflation rate as determined by the State Allocation Board. No other school requirements are necessary to adequately serve the new students in the Plan Area.

4.11 LIBRARIES

4.11.1 EXISTING CONDITIONS

The City of El Monte has two public libraries, the El Monte Library and the Norwood Library. Both libraries are operated by the Los Angeles County Public Library system. Both libraries offer adult and teen programs, summer reading programs for children, facilities for events, and many publications in English and other languages.

The El Monte Library is located at 3224 Tyler Avenue, approximately one mile south of the Plan Area. The library is a 12,000-square foot facility with more than 100,000 volumes. The Norwood Library is located at 3550 North Peck Road in El Monte approximately two miles north of the Plan Area. The library features a 10,000 square-foot facility that houses more than 90,000 volumes. Previous Figure 4-7 maps the location of the libraries in the City.

4.11.2 LIBRARY REQUIREMENTS

The increase in population resulting from the Specific Plan would increase use of available libraries. The Los Angeles County Board of Supervisors oversees implementation of an annual dedicated funding stream for the County's vast library system. This library system was established under authority of the County Free Library Act and a special fund department operating under the authority of the County Board of Supervisors and managed by a 20-member appointed Library Commission. It is one of the largest library systems in the country with a 7.5 million volume book collection serving over 3.5 million residents throughout 51 of the 88 incorporated cities of the County. Any population growth or decrease associated with the Specific Plan should be adequately served by this existing system. No additional requirements are needed to adequately provide library services to the new residents of the Plan Area.

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DOWNTOWN
MAIN STREET

Chapter 5
IMPLEMENTATION



Chapter 5 - Implementation

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5.2	Attracting Private Investment and Public Funding Benefits	5-2
5.3	Key Development Opportunity Sites	5-3
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5. IMPLEMENTATION

5.1 INTRODUCTION

This chapter identifies strategies and actions that are necessary to implement the desired community benefits and public improvements identified within the Specific Plan. Implementation requires a coordinated program of public and private action. Public improvements are especially important, as they add value to improving the Downtown area and lay the foundation for future private investment, in a sense “priming the pump”, and inspiring property owners, merchants, and investors to do the same. Each action will be guided, and in some cases carried out, by various City departments over time and included in the capital improvements program in an effort to ensure that community benefits and public infrastructure improvements remain a part of the on-going operation of the City. In some cases further study and analysis may be required to find realistic and timely solutions to implementation recommendations.

Each of the actions identified within this chapter and throughout the Specific Plan document will be carried out by the private sector as development occurs over time and by the City through additional policy initiatives, regulatory governance, administrative programs, and capital investment. Monitoring of the effectiveness of the implementation program will be conducted commensurate with the City’s regular budget cycle. The Implementation Action Plan (including the Development Opportunity Reserve Public Improvement List) contains a summary of actions proposed within the Specific Plan and provides direction related to the responsible parties, timing and cost associated with the item.

Implementation and investment strategies further developed within the Specific Plan include:

- a. Customized allowable land uses and development standards to assist in achieving a higher quality of development thus improving long-term investment values (**Sections 2.3 through 2.7**).
- b. The Development Opportunity Reserve (DOR), which rewards creative and high quality projects that invest in identified public infrastructure and community

benefit improvements, consolidation of smaller lots, and supporting the goals of the Specific Plan by allowing some flexibility in meeting the development standards and guidelines required in the Specific Plan (**Sections 2.2.2 through 2.2.4**). Flexibility to achieve the goals is a key ingredient in an incentive based plan. Incentives to be considered include increased residential density, additional commercial square footage (FAR), flexibility in development standards, streamline processing, and others. This process is outlined in Chapter 6 Administration and provides clear directions and findings that must be met in order to earn flexibility in standards.

- c. Coordinating with developers to develop Key Opportunity Sites. **Section 5.3** discusses several key development sites and makes recommendations as to the type of development and relative density that could be accommodated. The City will take a proactive role to guide and work with new development applications by providing leadership in the review and comment on submittals, and assist in application of incentives as outlined in this Specific Plan, to implement the vision and goals of this Specific Plan. In general, applicants should work proactively to ensure that resulting development meets or exceeds the expectations enumerated in this Specific Plan and represents a valuable long-term investment for the City of El Monte.

5.2 ATTRACTING PRIVATE INVESTMENT AND PUBLIC FUNDING BENEFITS

An effective Specific Plan typically involves both the public and private sectors in the plan implementation. Whereas development of the specific allowable land uses and creation of selected development standards envisioned for a plan area is often initiated by the public sector, the ultimate goal of this type of planning effort is to attract desired private investment. Broadly speaking, the following details two major ways that a municipality can facilitate desirable private development.

1. By creating a “conducive development environment” that is consistent with prevailing market demand for various land uses. This may include the following type of actions or policies:
 - a. Zoning that is responsive to market needs;
 - b. Allowing increased residential density and floor area for commercial projects through specialized “form and standard based” development standards to encourage lot consolidation and redevelopment of vacant, underutilized, and deteriorated properties;
 - c. Creating incentives in the form of new zoning, streamlined permitting, increased residential density, additional commercial square footage (FAR), and flexibility in development standards through a new development permit process that rewards creative projects that support the vision and goals of the Specific Plan;
 - d. Area-wide amenity investments, including signage, landscaping, and streetscape improvements;
 - e. Marketing of the Specific Plan area to consumers, developers, and prospective business tenants;
 - f. Technical assistance to Specific Plan area businesses; and
 - g. Enhanced code compliance to improve the visual appeal and function of the urban environment.
- A key advantage to adopting a Specific Plan and certifying the Environmental Impact Report (EIR) is that it provides a vehicle for expedited approval of development proposals that are consistent with the community vision established by the Specific Plan. Developers consistently cite this type of provision as a key factor in selecting the communities where they will pursue projects. It is essential that the adopted plan reduce the need for discretionary approvals for projects that fall within the development ‘envelope’ established by the Specific Plan and streamline the subsequent CEQA requirements.
2. By providing direct or indirect financial incentives to area businesses, property owners, and key development projects. This can involve the following types of initiatives:
 - a. Waiving or reducing various local fees and taxes;
 - b. Providing loans and/or grants for various business and property improvement purposes such as building façade renovations;
 - c. Investing in site- or projects-specific infrastructure;
 - d. Assisting in relocation of existing businesses; and
 - e. Creating ‘opportunity sites’ by making City-owned properties in the Specific Plan area available for appropriate development.

5.3 KEY DEVELOPMENT OPPORTUNITY SITES

A goal of the Specific Plan is to identify development opportunities within the study area that would help to catalyze economic growth and revitalization. Thus, along with a series of City initiated public improvements, corresponding private development opportunities are also recommended. The Specific Plan identifies two key opportunity sites for new mixed-use development and structured parking located on City-owned public parking lots. These sites provide an opportunity to repurpose existing surface lots to incorporate structured parking,

public facilities and amenities, or provide additional housing opportunities that will increase the number of residents in the area to establish a 24 hour/7 day a week enlivened Downtown environment. The following case studies represent potential development opportunities to be explored for City-owned sites identified in *Figure 5-1* and described in further detail on Pages 5-4 and 5-5. These case studies are highly conceptual in nature and are only provided as illustrations of future potential. Any future project proposed on these sites will require staff review, public comment, and public hearings before any decision making occurs.

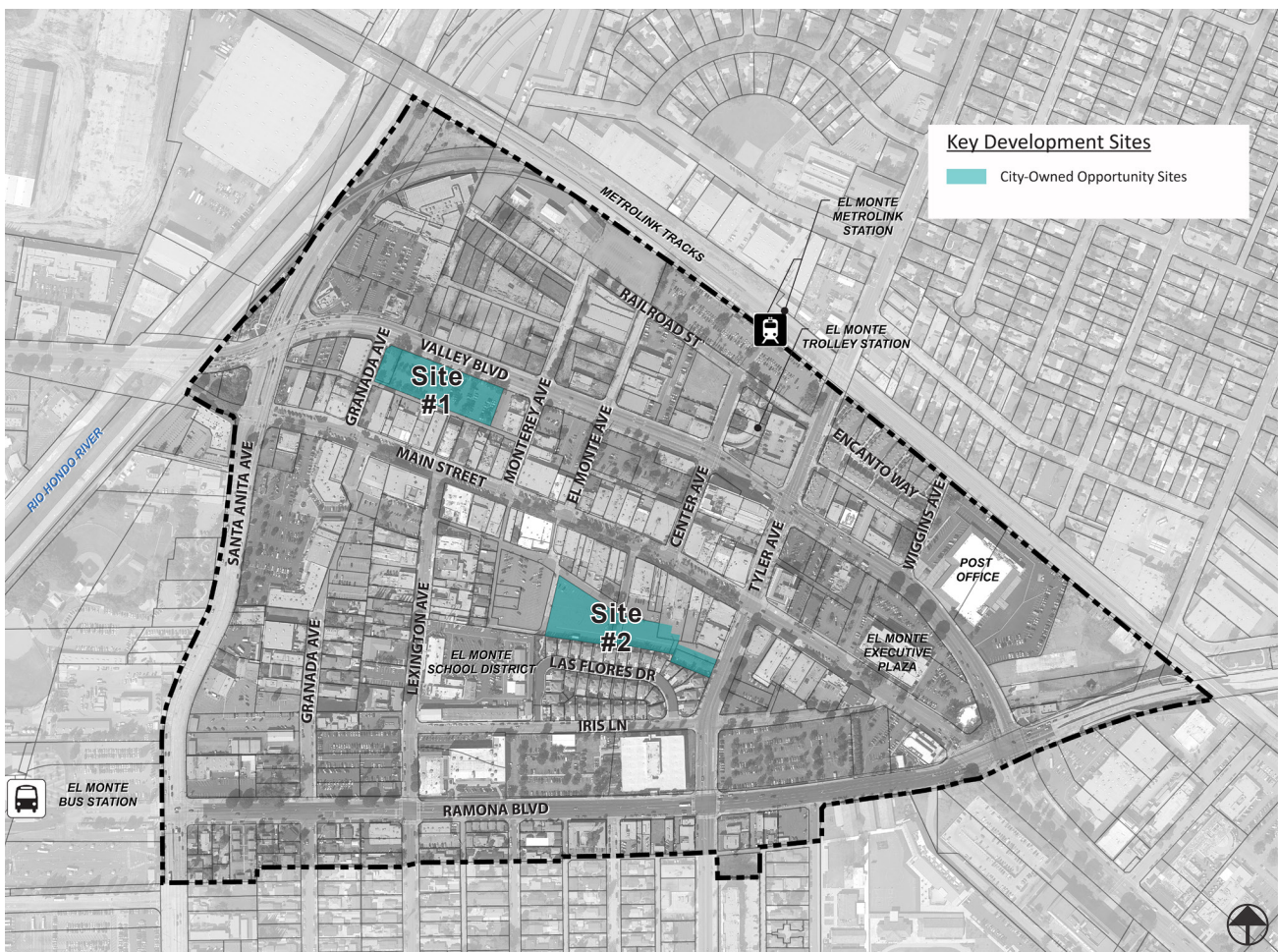


Figure 5-1 City-Owned Opportunity Sites

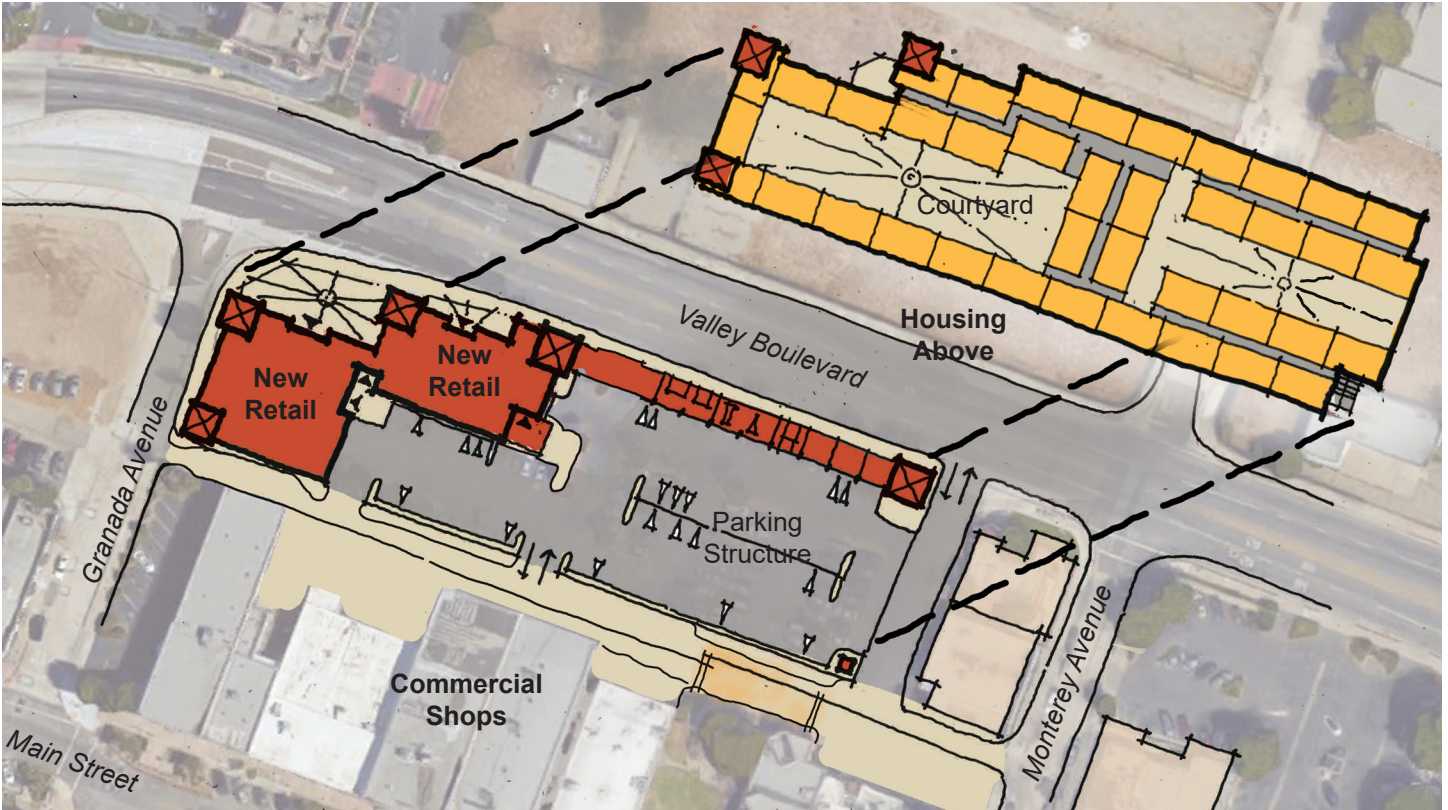
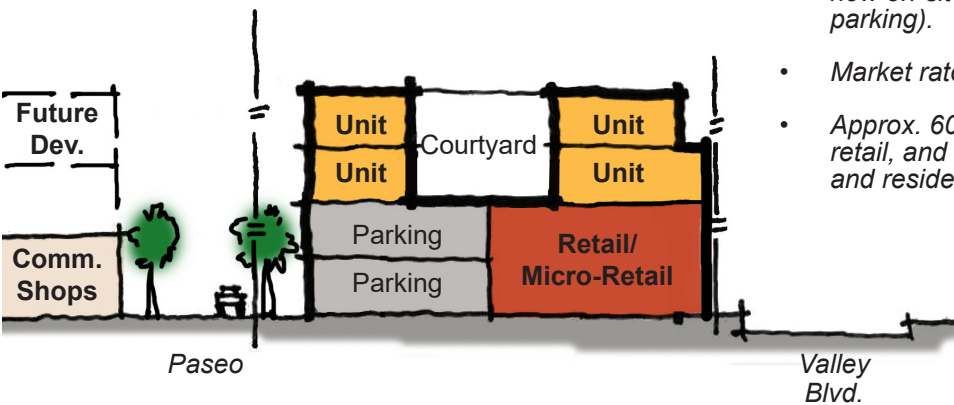


Figure 5-2 Mixed-Use Case Study

Site #1: New Mixed-Use Development

- Concentrated housing within a short walk to transit and Downtown.
- Places people in Downtown at night.
- New retail opportunities - both traditional and "micro-retail" (offers small space for non-standard retail such as artisanal foods, niche electronics, or curated boutiques).
- Parking located in a structure, replace some existing public parking as well as parking for new on-site development (for a net increase of parking).
- Market rate housing - for sale/rent units.
- Approx. 60 units of housing, 18,000 square feet of retail, and structured parking spaces (both public and residential) with DOR.

MAIN STREET SUB-AREA MIXED-USE



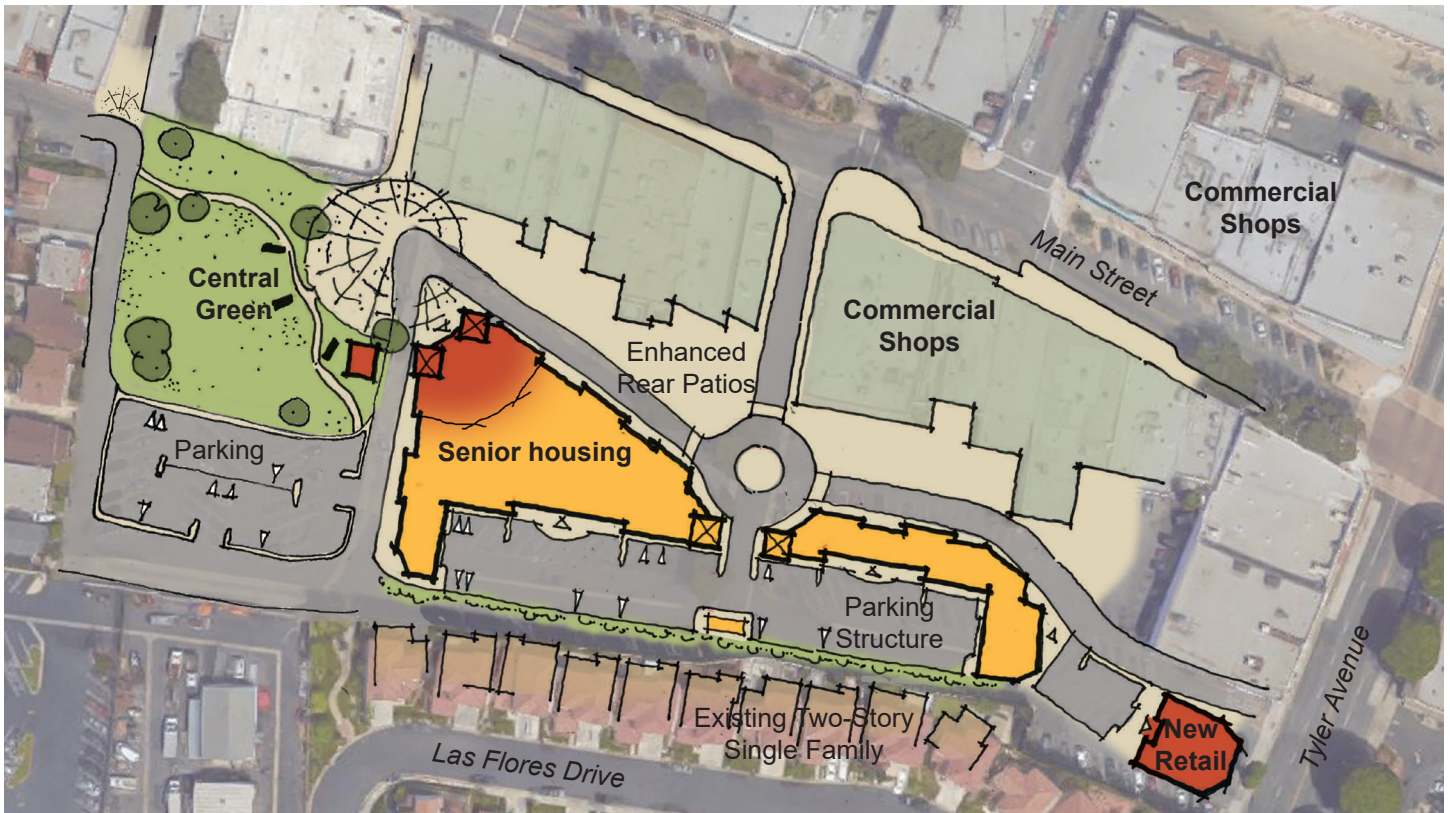
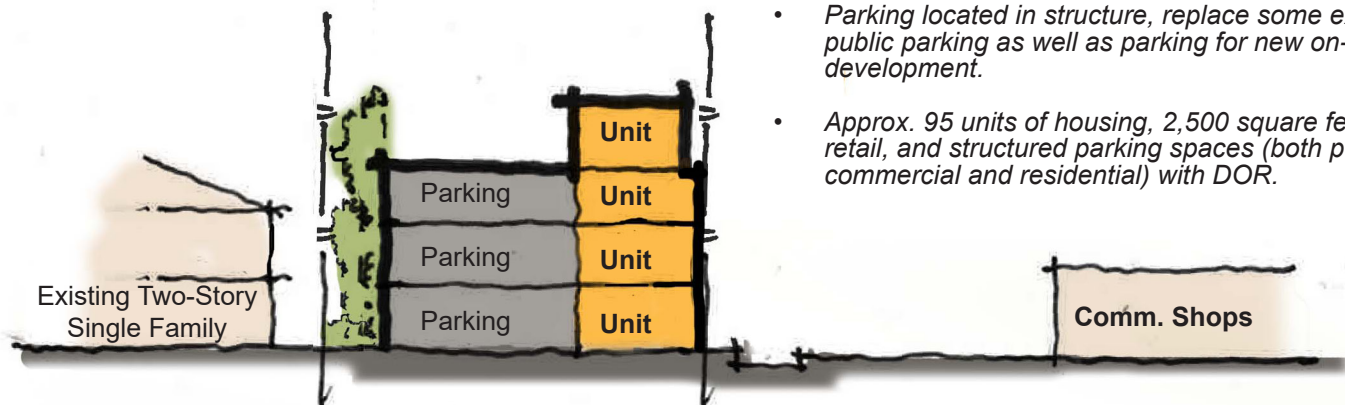


Figure 5-3 Senior Housing Case Study

Site #2: New Senior Housing Development

- Concentrated housing close to Downtown, medical services, and transit.
- Places people in Downtown at night.
- Activity and engagement within a public green space as well as new pedestrian space at rear of commercial shops.
- Green buffer between existing Single Family Development and new development.
- Parking located in structure, replace some existing public parking as well as parking for new on-site development.
- Approx. 95 units of housing, 2,500 square feet of retail, and structured parking spaces (both public, commercial and residential) with DOR.

ZÓCALO SUB-AREA SENIOR HOUSING



5.4 POTENTIAL FUNDING MECHANISMS

Several major categories of funding mechanisms are potentially applicable to the Specific Plan area:

1. Development Opportunity Reserve (DOR).
2. Formation of a Property Owner Business Improvement District (PBID) to fund various improvements and activities within the Main Street and Zócalo sub-areas.
3. General Fund allocations for planning efforts, capital improvements projects and program administration. General Fund revenues are monies collected by the city from property taxes, sales and use tax, transit occupancy tax, and other forms of revenue and are used by the City to fund municipal operations such as fire, police, development services, public works, recreation, and a wide variety of other municipal services.
4. Community Development Block Grant (CDBG) funds are another source of annual entitlement money that accrue to the City on annual basis for the purposes of investing in a variety of community projects from street improvements to façade rehabilitation to housing projects.
5. Transportation Enhancement Activities through SAFETEA-LU, a federal funding program focused on transportation enhancement such as street landscaping, alternative transportation, bike and pedestrian enhancements, street beautification efforts, etc.
6. Local gasoline tax revenues for improvements to local roads and streets. Can also bond these funds to capitalize them if desired.
7. Congestion management air quality funds for alternative transportation, bicycle and pedestrian enhancements and other programs and projects to improve air quality.

8. Lease-leaseback, lease revenue bonds, and/or general obligation bonds can be used as financing mechanisms by local government to pay for various public improvements.
9. Affordable Housing and Sustainable Communities (“AHSC”) Cap-and-Trade Funds.
10. Property Assessed Clean Energy (“PACE”) financing.
11. Site specific tax revenue pledges for revenue enhancing developments.
12. Enhanced Infrastructure Financing District (“EIFD”).
13. Community Revitalization and Investment Authority.
14. City Impact Fees (park in-lieu fees, public art fees, traffic impact fees, etc.).
15. Future grant opportunities.

5.5 IMPLEMENTATION ACTION PLAN

The vision and goals presented in the Specific Plan are supported by the following Implementation Action Plan. The Implementation Action Plan provides a summary of Specific Plan recommendations and is presented in a table format providing a clear listing of major programs, projects, and actions needed for implementation. The table also identifies the responsible agency or party and indicates a short, medium, or long-term priority for each item. A short-range priority indicates the item should be completed in the next year, while a medium-range priority indicates expected completion within the next two to five years. A long-term priority may take five or more years to complete.

In order to ensure that the Specific Plan is implemented, the Implementation Action Plan calls for establishment of an Implementation Task Force. This Task Force will be led by the City Manager or his/her designee, and could be composed of City Council members, Downtown business owners, local developers, Chamber, and/or DEMBA members.

TABLE 5-1 IMPLEMENTATION ACTION PLAN

Downtown Main Street Transit District Specific Plan Implementation Action Plan			
ACTION	TIMING	RESPONSIBILITY	
	1 = Short Range (0-5 Years) 2 = Mid-Range (6-10 Years) 3 = Long-Range (11+ Years)	CM = City Manager ED = Economic Development PW = Public Works RC = Recreation and Community Services CON = Consultant	
REGULATORY ACTIONS			
Adoption of Specific Plan: Adoption of the Specific Plan puts into place new zoning, development standards, permitting process and other proactive policies designed to spur economic investment and visual enhancement for Downtown.	1	ED	CON
Development Opportunity Reserve, Public Infrastructure In-Lieu Fee Program, and Parking In-Lieu Fee Program: Create an incentives package to spur development investment with elements such as: permit streamlining, land assembly, fee adjustments, and in-lieu fee programs.	1	ED	CON
Enhance Code Enforcement: Adopt a proactive code enforcement program focused on signage and beautification.	1	ED	-
IMPROVEMENT PROJECTS/DOR PUBLIC IMPROVEMENT LIST			
<i>Streets, Circulation, Intersections (ST) (see Sections 3.3 through 3.6)</i>			
Rename Valley Mall to Main Street.	1	PW	ED
Main Street and Santa Anita Avenue Improvements (see Figures 3-8 through 3-10).	3	PW	CON
Real-Time Signal Timing on Santa Anita Avenue, for peak hour congestion management.	2	PW	CON
Intersection Improvements at five-way Ramona Boulevard/Valley Boulevard/Main Street	1	PW	CON
Intersection redesign, Ramona Boulevard/Main Street, right turn in and out only.	1	PW	CON
Intersection Improvement/Multimodal Crossing, Santa Anita Avenue/Main Street, right turn in and out only.	2	PW	CON
Incorporate pedestrian safety crossing enhancements at the intersections of Valley Boulevard and El Monte Avenue and Lexington Avenue and Ramona Boulevard.	2	PW	CON
Integrate pedestrian safety crossing enhancements at Tyler Avenue and the Metrolink railroad line that include Metrolink’s newest grade crossing safety standards.	2	PW	CON
Incorporate pedestrian/bicycle priority crossings at the intersections of Santa Anita Avenue and Main Street, Santa Anita Avenue and Ramona Boulevard, Ramona Boulevard and Tyler Avenue, Tyler Avenue and Main Street, Valley Boulevard and Center Avenue, and the five-way intersection at Valley Boulevard, Main Street, and Ramona Boulevard.	2	PW	CON
Santa Anita Avenue complete street improvements, accommodating pedestrians, bicycles, automobiles, and public transit.	3	PW	CON

Downtown Main Street Transit District Specific Plan Implementation Action Plan

ACTION	TIMING 1 = Short Range (0-5 Years) 2 = Mid-Range (6-10 Years) 3 = Long-Range (11+ Years)	RESPONSIBILITY CM = City Manager ED = Economic Development PW = Public Works RC = Recreation and Community Services CON = Consultant	
		Lead	Support
<i>Bicycle Facilities (BF)</i> (see Sections 3-4 and 3-6)			
Bicycle rack installation on Main Street in groups of three on each side of the street between Tyler Avenue and Santa Anita Avenue.	2	PW	CON
Bicycle Facility on Tyler Avenue (i.e. a bicycle café).	3	PW	CON
Improved pedestrian/bicycle portal entries at the Rio Hondo including additional signage, lighting, and pavement markings.	3	PW	CON
Class II (Striped) bicycle lane on Valley Blvd, Tyler Avenue north of Ramona Boulevard, and portions of Ramona Boulevard.	2	PW	CON
Class II (Buffered) on Santa Anita Avenue, north of Valley Boulevard.	2	PW	CON
Class III lanes with sharrow (shared lane) stencil markings on roads designated in Figure 3-6.	2	PW	CON
Class III Bike Boulevards on El Monte Avenue, Lexington Avenue, and Main Street (Rio Hondo Bike Path to the intersection of Main Street/Ramona Blvd).	2	PW	CON
Bicycle crossing improvements as identified in Section 3.3.2.	2	PW	CON
Metro Bike Hub program, currently located at the El Monte Bus Station, also placed at the El Monte Metrolink Station.	3	PW	CON
<i>Parking (PK)</i>			
Improve existing public parking lots with new paving, reconfigured striping, increased landscaping, lights, and signage (Note: Parking Lot #1 on Valley Boulevard, east of Granada Avenue, is currently underway).	1	PW	ED
Public parking structures/garages to support Main Street shopping. This could replace existing surface parking lots provided there is a net increase in the number of public spaces.	3	ED	CON
Coordinate with Metro on needs assessment for a parking structure/garage to support the El Monte Metrolink Station to meet current SCRRA standards.	3	ED	CON
<i>Open Space, Recreation, Public Facilities (OS)</i> (see Sections 3.3)			
New public plaza at Metrolink Station.	3	RC	CON
New public plaza at southwest corner of Valley Blvd/El Monte Ave.	2	RC	CON
New public plaza within the Zócalo Sub-Area, south of Main Street	3	RC	CON
Improve plaza north of Main Street, at Lexington Avenue alignment and incorporate additional amenities.	2	RC	CON

Downtown Main Street Transit District Specific Plan Implementation Action Plan

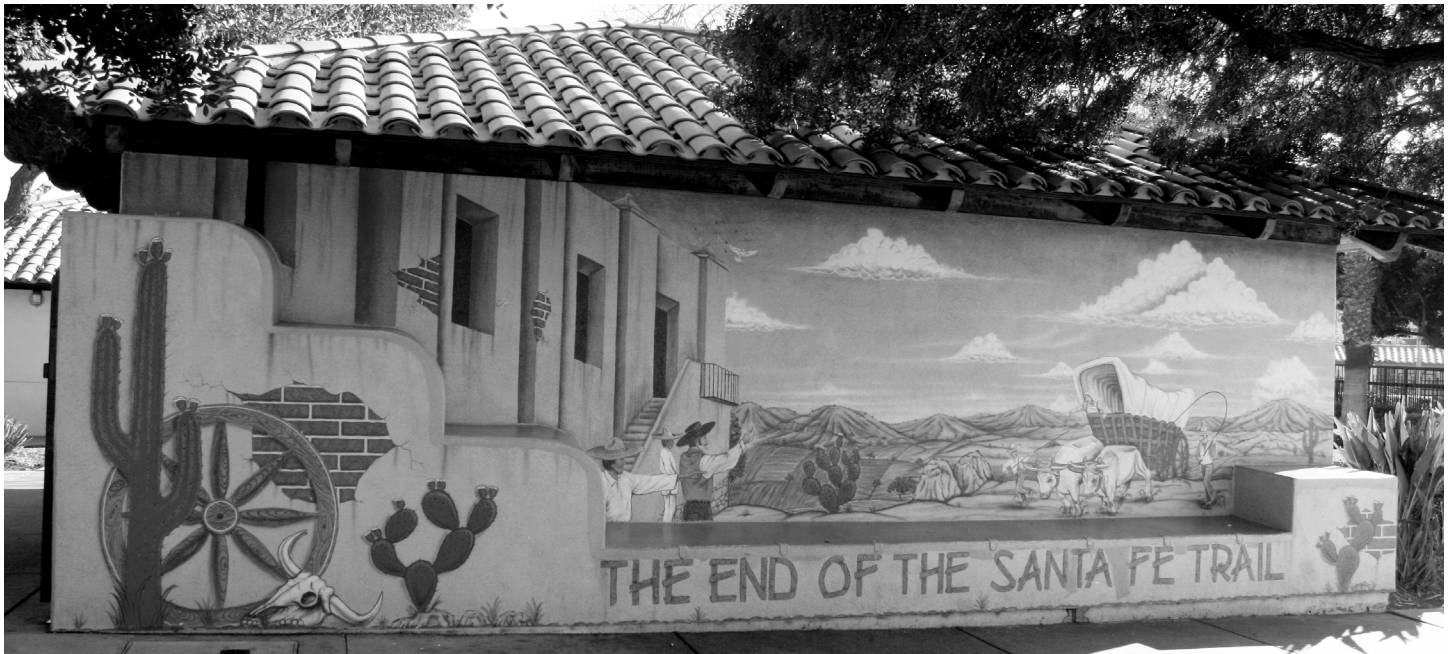
ACTION	TIMING 1 = Short Range (0-5 Years) 2 = Mid-Range (6-10 Years) 3 = Long-Range (11+ Years)	RESPONSIBILITY	
		Lead	Support
CM = City Manager ED = Economic Development PW = Public Works RC = Recreation and Community Services CON = Consultant			
Beautification, Street Furniture, Streetscape, Sidewalks, Paseos (BT) (see Sections 3.3, 3.5, and 3.6)			
Convert Palm Court into a paseo and provide an alley paseo to link Palm Court and Lexington Avenue.	3	RC	CON
Transform El Monte Avenue into pedestrian focused promenades linking the Metrolink Station to the Downtown core.	2	PW	CON
Enhance the existing pedestrian paseo near El Monte Avenue/south of Main Street with special paving, pedestrian amenities, and landscaping.	2	RC	CON
Santa Anita Avenue beautification, landscaping, public art, lighting.	3	PW	CON
Area One Streetscape Beautification enhancements (Figure 3-11) consistent with Section 3.5.	2	PW	CON
Area Two Streetscape Beautification enhancements (Figure 3-11) consistent with Section 3.5.	2	PW	CON
Pedestrians Lighting & String Lighting, and Decorative Lighting, at Main Street, Lexington, El Monte Ave., Central Ave. and Tyler Ave.	2	PW	CON
Decorative pavement treatments – at Main Street, Lexington, El Monte Ave., Central Ave., and Tyler Ave.	3	PW	CON
Signage (SN) (see Sections 3.7)			
New Signage, Change “Valley Mall” to “Main Street”.	1	ED	CON
Freeway Sign at Santa Anita off-ramps (“Downtown El Monte” next exit) – Caltrans coordination required.	2	ED	CON
Downtown logo - refined final graphic imagery depicting actual fonts, artwork, sign composition and materials using a signage consultant.	1	ED	CON
Entry/Monument Signs for Downtown at Ramona Blvd/Santa Anita, Valley Blvd/Main Street/Santa Anita Ave, Ramona Blvd/Main Street/Valley Blvd.	2	ED	CON
Directional Signage - transit, shopping, public plaza, government, freeway, parking, Rio Hondo bike path, etc.	2	ED	CON
Information Signage - Interpretative signs to highlight key buildings, pedestrian directories, public parking, Metro Bike Hub, and facility locations.	2	ED	CON
Banner signs, special event sign holders on Main Street and Lexington Avenue	2	ED	CON
Electronic/mobile app for wayfinding.	2	ED	CON
Wayfinding kiosks, four (4) locations.	2	ED	CON

**Downtown Main Street Transit District Specific Plan
Implementation Action Plan**

ACTION	TIMING	RESPONSIBILITY	
	1 = Short Range (0-5 Years) 2 = Mid-Range (6-10 Years) 3 = Long-Range (11+ Years)	CM = City Manager ED = Economic Development PW = Public Works RC = Recreation and Community Services CON = Consultant	Lead
<i>Transit (TR) (see Sections 3.2 and 3.6)</i>			
Improve Ramona Transit Plaza, at southeast corner of Ramona Blvd/Tyler Avenue, with streetscape treatments, landscaping, and wayfinding signage.	2	PW	CON
Replace/upgrade bus shelters for Downtown area consistency with trash receptacles, transit schedules, and lighting.	2	PW	CON
Explore future opportunities with Metro for relocating the El Monte Transit stop at Santa Anita Avenue on Ramona Boulevard to the El Monte Bus Station.	2	PW	CON
PROGRAMS			
Implementation Task Force: Establish a task force of key City staff and other stakeholders to oversee implementation of the Action Plan.	1	CM	ED
Business Improvement District: Evaluate and establish a property owner and Business Improvement District (PBID) for the Main Street and Zócalo sub-area.	2	ED	CON
Main Street Program: Investigate the opportunities and advantages of the National Main Street Program for the Specific Plan area.	1	ED	-
Parking Management Program: Develop a parking management program. Implementation items may include the establishment of an in-lieu fee, shared parking agreement processes, and Downtown parking fees.	2	ED	CON
Public Art and Murals: Establish a program to use funds from the Art in Public Places Fund to establish public murals and public art throughout the Downtown area.	1	ED	-
Cultural Arts Program: Investigate the creation of a Cultural Arts Program within the Downtown - including guidelines and allowances for the inclusion of Cultural Arts events, whether held on a regular basis or only temporary in nature, within public open spaces.	1	ED	-
Façade Improvement Program: Create a building façade improvement grant program.	1	ED	CON
PRIVATE KEY CATALYST PROJECTS (SHORT-TERM)			
Catalyst Project # 1: Solicit interested developers to assist new development of a senior housing project within the Zócalo sub-area.	3	ED	-
Catalyst Project # 2: Solicit interested developers to assist new development of a mixed-use market rate development within the Main Street Sub-area south of Valley Boulevard.	3	ED	-

DOWNTOWN
MAIN STREET

Chapter 6
ADMINISTRATION



Chapter 6 - Administration

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6. ADMINISTRATION

6.1 INTRODUCTION

This chapter describes the process which will be used to administer the Specific Plan, including the review of development applications and amendments to the Specific Plan.

6.2 SPECIFIC PLAN AUTHORITY AND ADOPTION

To an extent, the range of issues that are contained in a Specific Plan is left to the discretion of the decision making body. However, all Specific Plans must comply with California Government Code Sections 65450 through 65457. These provisions require that a Specific Plan be consistent with the adopted General Plan for the jurisdiction in which the Specific Plan area is located. In turn, all subsequent development proposals such as tentative subdivision maps, site plans, improvement plans, and all public works projects must be consistent with the adopted Specific Plan.

Pursuant to California Government Code Section 65453, a Specific Plan may be adopted by resolution or by ordinance. Specific Plans adopted by ordinance effectively become a set of zoning regulations that provide specific direction to the type and intensity of uses permitted and may also define design expectations and standards. The El Monte Downtown Main Street Transit-Oriented District Specific Plan and Master Plan is a regulatory document adopted by ordinance. In any instance where the Specific Plan conflicts with the requirement of the City of El Monte Municipal Code (which includes Title 17, the Zoning Ordinance), the Specific Plan provisions will take precedence. Where the Specific Plan is silent on a topic, the Municipal Code requirements remain in force.

6.3 SPECIFIC PLAN ADMINISTRATION

Proposed developments within the Specific Plan Area will be reviewed pursuant to established development review processes within the Zoning Ordinance. For example, project design review will be conducted pursuant to Chapter 17.122 - Design and Minor Design Review, of the

Zoning Ordinance. In addition, proposed developments will be required to adhere to existing City of El Monte Municipal Code regulations and processes for other types of discretionary review, such as those for special use permits, variances, and subdivisions, as may be applicable.

A development permit application for a permitted use or regulated use as defined in Chapter 2, Table 2-3 (Permitted Uses) that meets the requirements of this Specific Plan and adheres to all other relevant regulations of the City of El Monte Municipal Code may be approved administratively by the Community and Economic Development Director in the manner and with the notice required for such applications by the Zoning Ordinance. Any development application that does not meet the Specific Plan requirements, employs the use of the incentives program in Chapter 5 (Implementation), or requires a special use permit shall be reviewed and considered by the Planning Commission and/or City Council.

6.4 PERMITTED LAND USES AND INTERPRETATIONS

A. Uses expressly prohibited:

1. A proposed use within the Specific Plan must expressly be listed as a permitted use per Table 2-3 (Permitted Uses), except as authorized per Subsection C below.
2. The express enumeration and authorization in the Specific Plan of a particular class of building, structure, premises or use in a designated zoning district shall be deemed a prohibition of such building, structure, premises or use in all zoning districts of a more restrictive classification, unless otherwise noted.

B. Procedures for interpretations:

1. The Community and Economic Development Director, in consultation with the City Attorney, shall have the responsibility and authority to interpret

the meaning and applicability of all provisions and requirements of the Specific Plan.

- 2. An applicant may request an interpretation of a provision or requirement of the Specific Plan. The request shall be in writing and may include any supporting information. The Community and Economic Development Director's decision shall constitute the precedent for all future interpretations of the provision or requirement. The Community and Economic Development Director also has the authority to initiate interpretations without a request from an applicant.

C. Additional permitted uses. Uses that are not specifically listed as a permitted use per Table 2-3 (Permitted Uses) may be permitted, provided the Community and Economic Development Director finds and determines all of the following:

- 1. The use is similar to other uses permitted in the Specific Plan;
- 2. The use will not have a negative impact related to traffic, noise, air quality, parking or public safety;
- 3. The use will not have a negative effect to the welfare of other businesses or individuals in the Specific Plan and surrounding area;
- 4. The applicant has submitted information that is substantial and compelling to support his or her request; and
- 5. The applicant has agreed to any conditions placed by the Community and Economic Development Director to ensure the use is not altered over time to become inconsistent with the original request and approval.

6.5 NON-CONFORMING USES AND STRUCTURES

Refer to Chapter 17.156 - Nonconforming Provisions, of the Zoning Ordinance for information on nonconforming uses and structures.

6.6 EXCEPTIONS

Certain minor adjustments to the Specific Plan development standards may be approved by the Community and Economic Development Director through the Administrative Permit process outlined in Chapter 17.120 of the Zoning Ordinance.

- 1. Reduce building separation requirements by 5', provided minimum Building and Fire Code requirements are met.
- 2. Increase in the permitted number of access points to a commercial or residential property.
- 3. An increase in building height up to 10' to allow for architectural features such as towers, elevator service shafts, and/or roof access.
- 4. Reduce or increase setbacks up to 20% or 5', whichever is greater in order to provide amenities to improve pedestrian circulation or architectural design.

Refer to Chapter 17.125, Variance and Minor Variance, of the Zoning Ordinance for all other adjustments to the Specific Plan development standards.

6.7 SPECIFIC PLAN BUILDING INTENSITY

As part of this Specific Plan effort, a CEQA review was completed for the potential development of a total net new pool of 2,200 net new residential dwelling units and 500,000 net new square feet of commercial, non-residential development within the Specific Plan Area. This maximum allowable potential development within the Specific Plan Area does not take into account environmental constraints or application of all

development standards. It is anticipated that buildout will take place over time and thus it is unknown when complete buildout of the Specific Plan Area will occur. In order to ensure that the cumulative buildout assumed for the overall Specific Plan Area is not exceeded, the Community and Economic Development Director will track the amount of net new residential units and commercial, non-residential square footage developed under this Specific Plan (by right and under the DOR), and will allocate Residential DOR Credits and Commercial DOR Credits until the pool is fully utilized or the pool is increased by subsequent CEQA study.

6.8 SPECIFIC PLAN AMENDMENTS

Refer to Chapter 17.130 - Specific Plans, of the Zoning Ordinance for the process to amend the Specific Plan.

6.9 DEVELOPMENT OPPORTUNITY RESERVE (DOR)

A. Purpose and Overview:

1. The intent of the DOR is to encourage increased development intensity concurrent with the delivery of public improvements or other community benefits to satisfy the increased demand for public amenities that come with the increased development intensity.
2. Each subarea within the Specific Plan has development standards on permitted maximum height, number of stories, nonresidential FAR and residential density by-right. A “development bonus” or DOR is also provided for in each sub-area. Through the DOR process, the applicant may increase the height, number of stories, nonresidential FAR and residential density, subject to the delivery of additional amenities by the applicant or payment of an in-lieu fee (Figure 2.2, Table 2-1).
3. The DOR supports an inventory of public improvements and amenities determined to be

necessary to support development intensity within the Specific Plan. The applicant may deliver the improvements and amenities or pay an in-lieu fee.

B. Types of DOR Credits. Increased levels of development intensity are permitted as consideration for the delivery of public improvements as measured by “Residential DOR Credit(s)” and “Commercial DOR Credit(s)”, collectively “DOR Credits”, as described below:

1. One (1) Residential DOR Credit permits the development of one (1) residential dwelling unit greater than the number permitted by-right, up to the maximum permitted under the Specific Plan DOR.
2. One (1) Commercial DOR Credit permits the development of 2,000 sf of nonresidential square footage greater than the number permitted by-right, up to the maximum permitted under the Specific Plan DOR.
3. Projects that exceed the height or number of stories permitted by-right, but do not exceed the number of residential dwelling units permitted by-right or the nonresidential square footage greater than the number permitted by-right. The minimum number of DOR Credits shall equal the number of residential dwelling units or nonresidential square footage that are located in the portions of the project that exceeds the height or number of stories permitted by-right. For example, if a property permits 50 dwelling units and a height of two (2) stories by-right, and an applicant proposes 50 dwelling units and a height of three (3) stories (allowed through the DOR process), the applicant will need to secure DOR credits for all the dwelling units that are fully or partially located on the 3rd floor.
4. The DOR Credit in-lieu fee shall be as adopted by the most recent City Council Fee Resolution.

C. General Procedures. The rules and procedures for applications, records, investigations applicable to applications for a DOR permit shall be as follows:

1. Application. Applications for a DOR permit shall be made on the appropriate form. The Community and Economic Development Director shall determine the minimum filing procedures, content and form of materials which must be submitted before the City can review and take action on the request. The filing procedures and applications shall be published and made available to the public. No petition shall be received unless it complies with all filing requirements. Refer to Section 17.10.080 of the Zoning Ordinance if more than one (1) application or action is necessary for the project.
2. Records. Applications filed pursuant to this Section shall be numbered consecutively in the order of their filing and shall be part of the permanent official records of the Planning Division.
3. Public Notice. Applications for a DOR permit do not require public notice.
4. Meetings and Decisions. Applications for a DOR permit do not require a public meeting. The Community and Economic Development Director shall issue a decision within 30 days of determining that the application is complete.
5. Time period. Approvals of a DOR permit shall be valid for one (1) year. If the applicant has proceeded in good faith towards the implementation of the project, as determined by the Community and Economic Development Director, the applicant may request a 12-month extension. The extension shall be considered within 30 days of the request. The applicant may request additional extensions, which shall be considered by the Planning Commission.

D. DOR Credits – Private Improvements. The applicant may propose the following improvements, amenities and incentives on private property to receive DOR Credits:

1. Affordable housing:
 - a. One (1) very low income unit shall be equal to 1.5 DOR Credit.
 - b. One (1) lower income unit shall be equal to 1.0 DOR Credit.
 - c. One (1) moderate income unit shall be equal to one-half (½) DOR Credit.
2. Arts or cultural space. One (1) DOR Credit for each 2,000 square feet of arts or cultural space.
3. Parcel consolidation. One (1) DOR Credit for each parcel consolidated into one (1) parcel with a minimum total area of 20,000 square feet. Therefore, if 4 parcels were consolidated to create one (1) parcel with an area of 20,000 square feet, the project will be eligible for 4 DOR Credits.
4. Parking spaces:
 - a. Privately accessible parking spaces. One (1) DOR Credit for every 6 parking spaces in excess of the minimum required per Chapter 17.70 – Parking Regulations, of the Zoning Ordinance.
 - b. Publicly accessible parking spaces. One (1) DOR Credit for every four (4) publicly accessible parking spaces in excess of the minimum required per Chapter 17.70 – Parking Regulations, of the Zoning Ordinance.
5. Public art:
 - a. One (1) DOR Credit when public art is installed on-site and the art has a minimum value of 0.5% of the total project cost; or

- b. Two (2) DOR Credits when public art is installed on-site and the art has a minimum value of 1.25% of the total project or exceeds the public art cap by more than 10% as adopted by the most recent City Council Fee Resolution.
6. Publicly accessible open space:
- a. Residential only projects. One (1) Residential DOR Credit for each 400 sf of publicly accessible open space, for a maximum of 5 Residential DOR Credits.
 - b. Nonresidential projects or mixed-use projects with residential:
 - i. Projects with 3,500 sf or less of nonresidential space. One (1) DOR Credit for each 400 sf of publicly accessible open space, for a maximum of 8 DOR Credits.
 - ii. Projects with more than 3,500 sf of nonresidential space. One (1) DOR Credit for each 400 sf of publicly accessible open space in excess of what is required in Chapter 2 of this Specific Plan, for a maximum of 10 DOR Credits.
 - c. Examples of open space. Examples include, but are not limited to, formal plazas, urban gardens, park/green space, open space and courtyard space.
7. Other publicly accessible improvements, amenities or other incentives. Other proposals to secure DOR Credits may be considered by the Community and Economic Development Director on a case-by-case basis. The Community and Economic Development Director may also refer the request to the Planning Commission for consideration.
- E. DOR Credits – Public Improvements. The applicant may propose improvements within the public right-of-way and on other City property to receive DOR Credits:
1. Calculations. The number of DOR Credits shall be based on the value of the public improvement(s) in comparison to the DOR Credit in-lieu fee. Therefore, if the value of the public improvement(s) is 4 times the DOR Credit in-lieu fee, the project will be eligible for four (4) DOR Credits.
 2. Process. The contract for the delivery of public improvements shall be awarded through a public bid process managed by the City.
 3. Underpayment. To the extent that the actual contact amount is less than the equivalent amount of DOR Credits required for the project, the applicant may deliver additional public improvements or pay the DOR Credit in-lieu fee.
 4. Overpayment. To the extent that the actual contact amount is greater than the equivalent amount of DOR Credits required for the project, the applicant may request a credit from the City for the difference. At the City's discretion, the City may reimburse the applicant with funds from the DOR Public Improvement Fund, or the City may wait for receipt of additional in-lieu payments from which it will reimburse the applicant. The applicant may not utilize the credits on another project without the expressed permission of the Community and Economic Development Director.
 5. Quality and design of improvements. The Community and Economic Development Director and Public Works Director must approve the design, plans and specifications for all public improvements delivered. All public improvements must meet or exceed City standards.

6. Timing to deliver the public improvements:
 - a. The work shall be completed prior to the issuance of building permits; or
 - b. A contract to complete the award shall be signed prior to the issuance of building permits. The applicant shall be responsible for maintaining a completion bond sufficient to cover the scope of the public improvements until the City has issued a certificate of completion for the public improvements. The cost of the competition bond may be included in the calculation of the contract amount and equivalent DOR Credits in-lieu fee.

F. DOR Credits – In-lieu Fee. The applicant may propose to pay an in-lieu fee to receive DOR Credits. Payments shall be due at time of building permit issuance. Payments shall go into a DOR Public Improvement Fund. Such funds shall only be used to support the delivery of public improvements on the DOR Public Improvements list within the Specific Plan.



Appendix A
DEFINITIONS



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A. LAND USE DEFINITIONS

Refer to Chapter 17.150 - Use Definition, of the Zoning Ordinance for all use definitions applicable to the Specific Plan.

B. GENERAL DEFINITIONS

The following terms have been included within Appendix A to aid users of the Specific Plan in understanding specific terminology used herein.

BICYCLE/PEDESTRIAN PRIORITY CROSSING - A multimodal intersection signed and signalized to give longer crossing times to pedestrians and bicyclists. Treatment solutions focus on safety given surrounding characteristics that vary from location to location.

MID-BLOCK PEDESTRIAN CROSSING - A designated crossing between signalized intersections with signage, striping, and stop-control devices to provide safe crossing for pedestrians.

PEDESTRIAN SAFETY CROSSING - A particular location with safety provisions for pedestrians traveling across a multi-lane roadway or other route. May be signalized. Locations are designated based on providing pedestrians with a more direct path of travel to and from destinations.

PEDESTRIAN/BICYCLE PORTAL - A well-identified entry way into a primary or secondary path of travel and/or an entry way into a bikeway, typically a Class I, dedicated bike path. Portals often provide community identity and serve as attractions.

ALL OTHER GENERAL TERMS – Refer to Chapter 17.152, General Definitions, of the Zoning Ordinance for all general definitions applicable to the Specific Plan.

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